

Chapter 6 DEVELOPMENT STANDARDS

The North Camino Ramon Specific Plan encourages mixed use development and flexibility of land use within a strong framework of public and private sector improvements to create a visually rich environment in support of economic development, community life, and smart growth. This Chapter defines the major elements of that framework by establishing standards for land uses, streets, sidewalks, and new development on individual blocks within the Plan Area. Adherence to these standards, combined with the design guidelines in Chapter 7, will allow development flexibility and economic growth while achieving a high quality environment with diversity and a unique community identity.

APPLICABILITY

The development standards contained in this section apply to new construction and significant additions and renovations to existing buildings. Existing development constructed or approved prior to the date of this Specific Plan's adoption by the City Council may retain existing land uses and remain as constructed. This allowance will not be affected by changes of ownership or new tenants continuing existing uses.

STREET FRAMEWORK

The street framework, shown in Figure 6.1, builds upon the current system of roadways, characterized by high traffic volumes, through the addition of new streets to provide more travel options, increased street frontages for development, and the encouragement of pedestrian travel to reduce vehicular traffic and the amount of land area devoted to parking. This framework will be implemented over time by coordinated public and private sector implementation efforts.

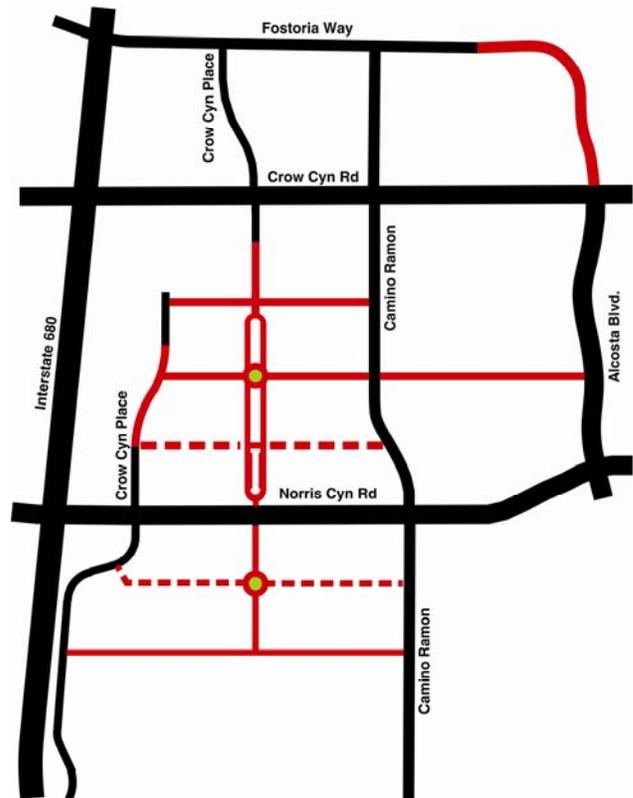


Figure 6.1: Street Framework
Red; Proposed Dashed; public or private with location flexibility.

NCRSP DRAFT-February 2012

DEVELOPMENT STANDARDS

The following development standards are applicable to the Plan Area as a whole. The standards are further refined by the individual block development and street standards.

Table 6.1 Development Standards		
Minimum Lot size:		
80,000 square feet, minimum 200 feet in width		<i>The minimum lot size associated with proposed development parcel and is if intended to maintain minimum parcels for purpose of coordinated development. Residential subdivision, condominium (residential and commercial) and post construction subdivision for the purpose of finance may deviate from this standard. Consolidation of small lots is encouraged; however, existing development on parcels smaller than the minimums may remain, may be upgraded consistent with the applicable development standards. Subdivision of parcels larger than the minimum, or development on a portion of a parcel, is subject to approval of a Development Plan for the entire existing parcel</i>
Building Setbacks: Commercial Mixed Use		
Front /Corner side –		See Street Sections for specific setbacks by streets If no setback is called out then: 6-foot minimum; when adjacent to road is less than 4 lanes: 10-foot minimum adjacent to roads with 4+ lanes and all arterials :
Interior Side / Rear		Daylight Plane Requirement Only(Zoning Ordinance) <i>Daylight Plane: No structure may intercept a 30-degree daylight plane inclined inward at a boundary line where residential development exists or has been proposed. The 30-degree daylight plane shall be measured from 8 feet above existing grade.</i> <i>Maximum setbacks or “build to lines are intended to shape the streetscape and ensure that building contribute to the streetscape</i>
Height Limit:		
85 feet or 5 stories, whichever is less.		FAR may place additional limitation on the heights based on site characteristics and proposed development
Floor Area Ratio:		
Maximum 0.70 average for project area.		Parking Additional floor area may be allowed on individual parcel at the discretion of the Planning Commission based upon the criteria set forth in the Floor Area Ratios discussion in Chapter 4-
Residential Density		
Minimum 20 Units per acre		For mixed use projects residential density must be calculated into the FAR calculation for the project.
Other Standards		
Landscape		Per Zoning Ordinance MU Standards Structured parking wrapped by buildings may be reduced to as low as 5% of the total lot area in the TCMU, and CMU Districts.
Parking		See Parking Requirements in Chapter 4.
Signage		See Signage Guidelines in Chapter 7

BLOCK DEVELOPMENT STANDARDS

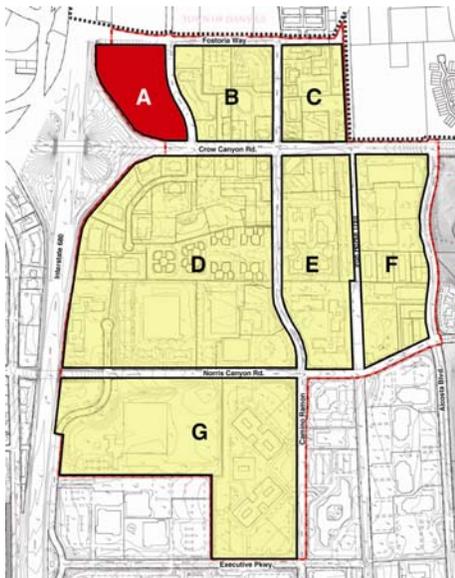
The following standards are specific to the individual Development Blocks within the Plan Area. The intent for each block is related to the Conceptual Plan and Vision statement. These standards provide additional requirements that are geographically specific, and should be used in conjunction with the street, sidewalk, and general standards set forth in other Chapters.

BLOCK A

Block A is currently developed with two mid-rise office structures with surface parking and a gasoline service station. Future development intensity is contingent on the use of structured parking.

Intent:

- Maintain the landscaped streetscape edge along Crow Canyon Place.
- Reinforce the Crow Canyon Road/Crow Canyon Place intersection corner.
- Locate future development adjacent to Crow Canyon Place to encourage pedestrian traffic and a relationship with uses on the adjacent blocks.



Internal Streets

None Required

Minimum Streetwall Heights:

35 feet.

Upper Floor Setbacks:

Daylight Plane Requirement

Public Spaces:

None Required

Applicable Street Sections:

4.10: Crow Canyon Road

Special Features:

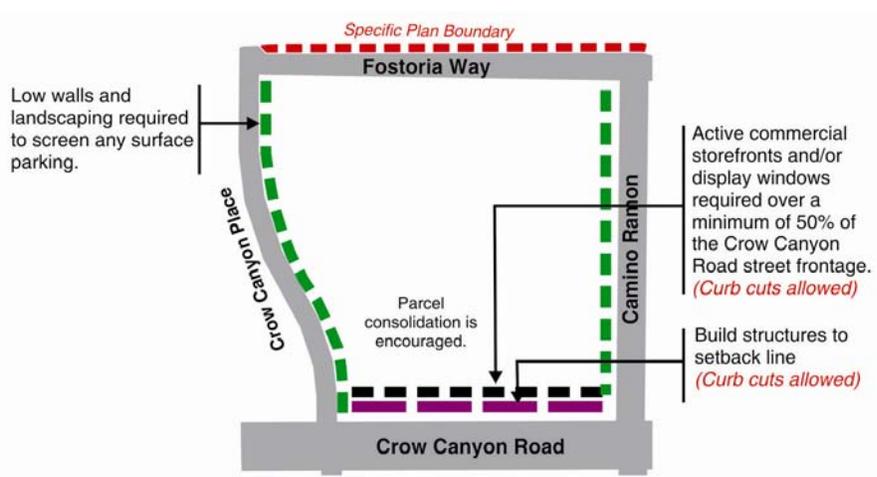
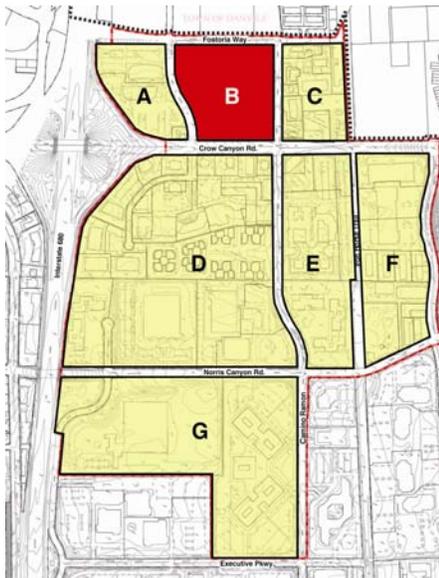
- Provide two-story structure at the Crow Canyon Road/Crow Canyon Place intersection.
- Reinforce the Crow Canyon Road/Crow Canyon Place intersection with architectural emphasis (e.g., tower element).
- Maintain pedestrian connection between office uses and adjacent shops and Services (Magnolia Square).

BLOCK B

Block B contains a wide variety of uses including retail shops, offices restaurants, and a hotel. Each is largely isolated from adjacent uses. Future development potential is enhanced by the attraction of nearby regional retail uses, but smaller parcels would likely need to be consolidated to achieve greater densities than their current condition.

Intent:

- Enhance City entry by development intensity along Crow Canyon Road.
- Encourage parcel consolidation to improve land development utilization.
- Maintain existing green street edges streetscapes.



Internal Streets:

None Required

Minimum Streetwall Heights:

35 feet

Upper Floor Setbacks:

Daylight Plane Requirement

Public Spaces:

None Required

Applicable Street Sections:

4.10: Crow Canyon Road

Special Features:

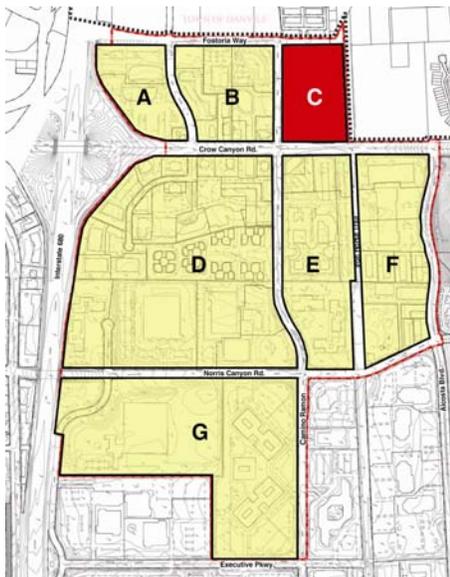
- Active commercial storefronts and/or display windows are required over a minimum of 50% of the Crow Canyon Road street frontage.
- Maintain pedestrian connection between office uses and adjacent shops and Services (Magnolia Square).

BLOCK C

Block C currently contains a wide range of commercial uses, including service commercial uses. An under-utilized parcel, owned by the San Ramon Unified School District, offers major potential for new uses and structures to better define the Crow Canyon Road entry to the City. Many of the service commercial uses will remain for some time, until time and future opportunities for other suitable sites within San Ramon open up.

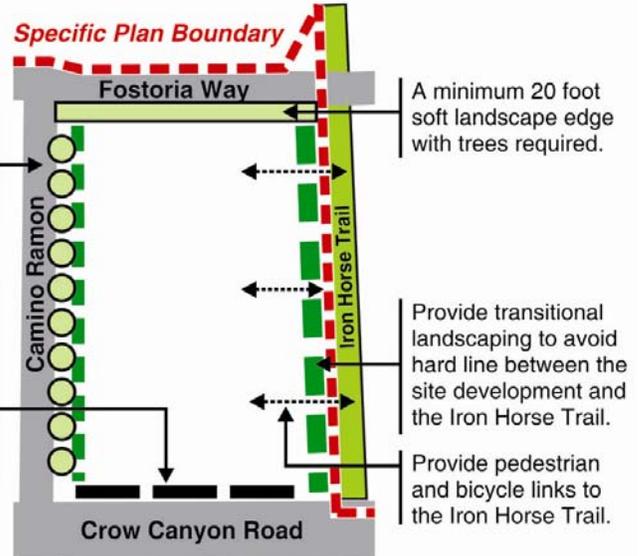
Intent:

- Enhance the Crow Canyon Road frontage.
- Encourage parcel consolidation over time for better land utilization.
- Accommodate continued service commercial uses.
- Mitigate new development impacts on adjacent residential development.
- Maintain the landscaped streetscape edge along Crow Canyon Place.
- Provide a strong interface with the adjacent Iron Horse Trail.



Low walls and landscaping required to screen any surface parking.

Active commercial storefronts and/or display windows required over a minimum of 50% of the Crow Canyon Road street frontage. *(Curb cuts allowed)*



Internal Streets:	None Required
Minimum Streetwall Heights:	35 feet
Upper Floor Setbacks:	Daylight Plane Requirement
Public Spaces:	None Required
Applicable Street Sections:	4.10: Crow Canyon Road
Comments/Special Features:	

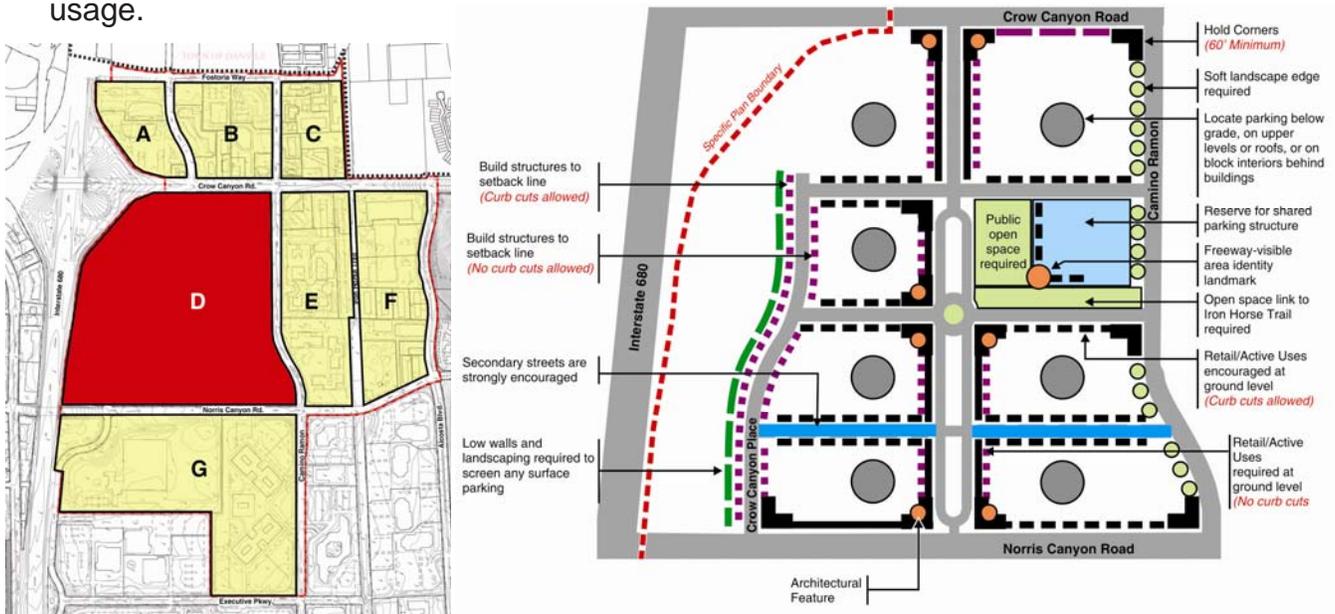
- Active commercial storefronts and/or display windows are required for a minimum of 50% of the Crow Canyon Road street frontage.
- Consider entry and pedestrian orientation along the Iron Horse Trail to strengthen connectivity and create a sense of place along the trail.

BLOCK D

Block D is the heart of the North Camino Ramon Specific Plan Area, and the original core area of the City. Its large size, adjacency to the Crow Canyon Road entry to San Ramon, and adjacency to Bishop Ranch and the City Center will allow it to evolve over time into a strong mixed use core with a strong emphasis on transit usage and shared parking. The block's planned circulation system will encourage a strong pedestrian environment.

Intent:

- Encourage and accommodate mid and large floor plate retail uses within a pedestrian retail environment.
- Establish a strong land use and visual sense of place.
- Encourage mixed use development.
- Encourage residential development to support nearby, commercial uses, Bishop Ranch employment, and City Center.
- Closely integrate public parks and open spaces with the commercial development.
- Provide unique focal points for public uses.
- Maximize pedestrian movements while subordinating the impact of vehicular parking.
- Encourage greater transit usage.



Internal Streets:

Provide linkage between the north and south segments of Crow Canyon Place.

Provide two new east-west streets north of the current City Hall.

Strongly encourage new east-west streets south of the current City Hall.

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Minimum Streetwall Heights:	35 feet
Upper Floor Setbacks:	Daylight Plane Requirement
Public Spaces:	Park Commons, Park Green Iron Horse Trail Link, Pocket Parks and Paseos are encouraged.

Applicable Street Sections:

4-1: Park Commons North

4-2: Park Commons

4-3: Crow Canyon Place

4-4: Street A

4-5: Street B West

4-6: Street B East

4-7: Street C

4-10: Crow Canyon Road

4-11: Norris Canyon Road

4-12: Camino Ramon North

4-17: Service Corridor

Comments/Special Features:

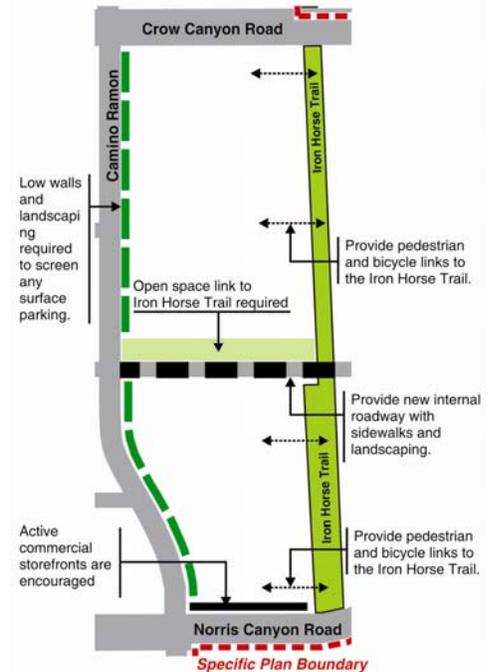
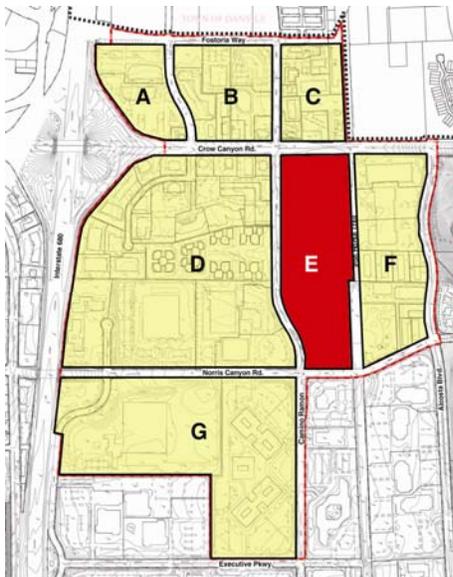
- Active commercial storefronts and building entries are required on the following street frontages: Crow Canyon Road, Park Commons North, Park Commons, Crow Canyon Place, Streets A, B and C, Norris Canyon Road.
- Build-to Lines(maximum setback) required for Park Commons, Crow Canyon Road, Crow Canyon Place, Norris Canyon Road, Streets A, B and C.

BLOCK E

Block E currently contains only three uses - the PG&E Conference Center, Bishop Ranch 11, and a single office building. Its large parcels offer substantial future opportunities for increased development intensification and integration of open spaces with the adjacent Iron Horse Trail.

Intent:

- Encourage the intensification of the northern portion of the block to an integrated office campus with substantial open spaces and on-site amenities.
- Encourage the consolidation of Bishop Ranch 11 offices with other offices on Block G with the southern portion of the block devoted to high quality, high density multifamily residential development to provide housing to support the employment base in the area, and to provide residents to support the retail uses in the City Center and Park Commons.



Internal Streets:

Provide a new Street D West from Camino Ramon to align with the future Street D East to link Camino Ramon to Alcosta Blvd.

Minimum Streetwall Heights:

35 feet

Upper Floor Setbacks:

Daylight Plane Requirement

Public Spaces:

Iron Horse Trail Link

Applicable Street Sections:

4-7: Street D West

4-11: Norris Canyon Road

4-10: Crow Canyon Road

4-12: Camino Ramon North

Special Features

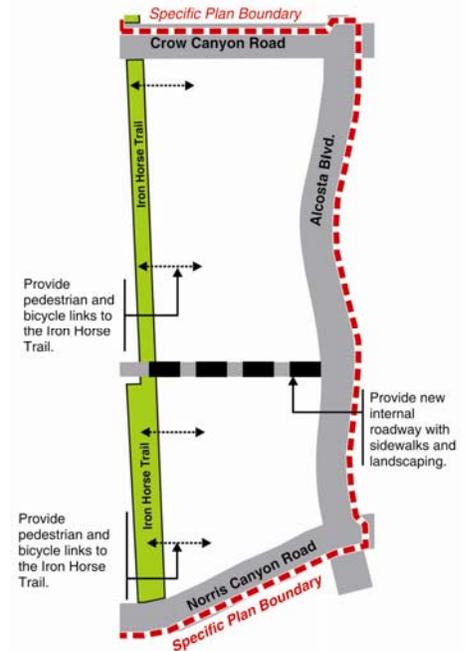
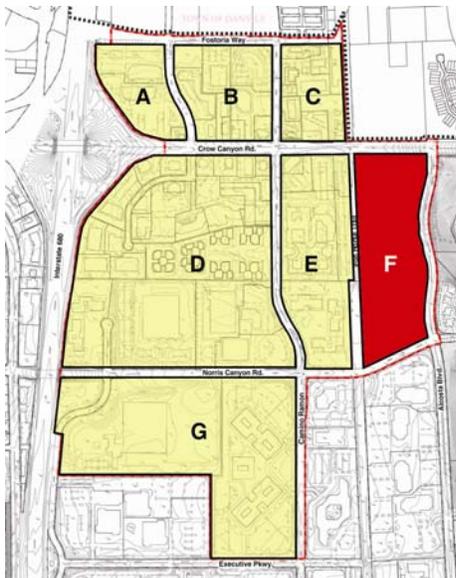
- Provide pedestrian and bicycle links to the Iron Horse Trail.
- Integrate the development with the Iron Horse Trail.
- Active commercial storefronts and building entries are encouraged on Norris Canyon Road.

BLOCK F

Block F contains many small parcels which will be difficult to develop at densities that are greater than currently exist. In addition, topography differences between Alcosta Blvd. and the properties in the northern portion of the block present special development challenges. A U.S. Post Office is currently located on this block.

Intent:

- Encourage small parcel consolidation to allow more integrated development of the area over time.
- Encourage high quality multifamily residential development to provide housing to support the employment base in the area, and to provide residents to support the retail uses in the City Center and Park Commons.
- Encourage a strong relationship to uses on Block E.



Internal Streets:

Provide a new Street D East from Alcosta Blvd. to align with the future Street D West to link Alcosta Blvd. to Camino Ramon.

Minimum Streetwall Heights:

None Required

Upper Floor Setbacks:

Daylight Plane Requirement

Public Spaces:

None Required

Applicable Street Sections:

- 4-8: Street D East
- 4-10: Crow Canyon Road
- 4-11: Norris Canyon Road

Special Features

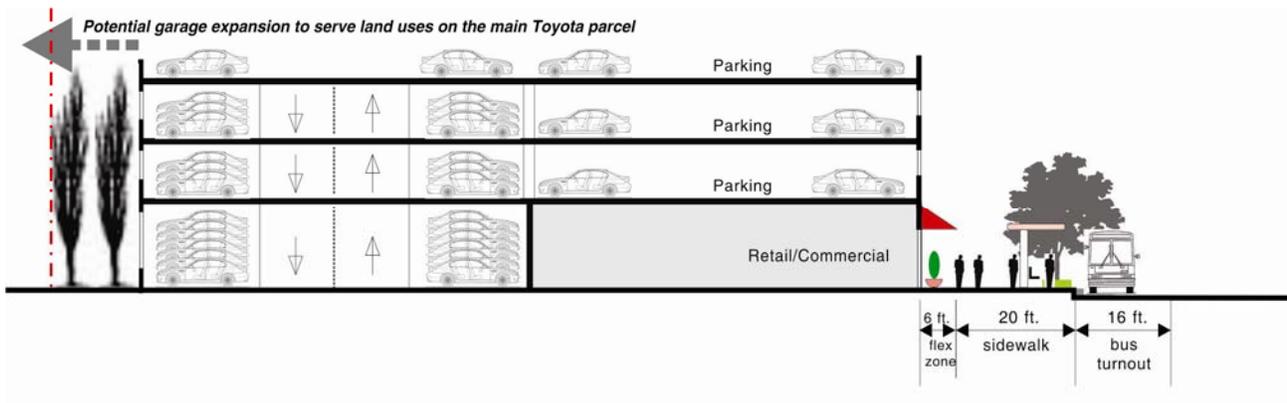
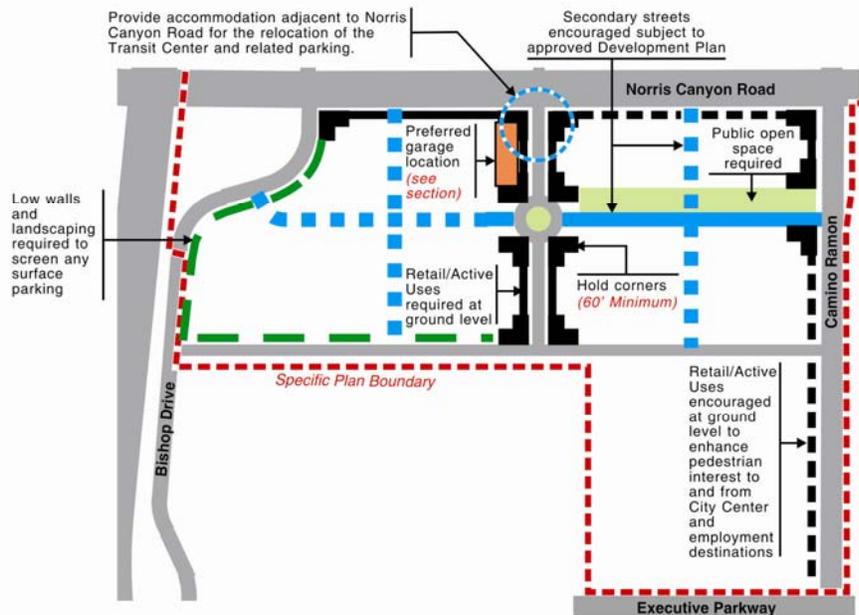
- Provide pedestrian and bicycle links to the Iron Horse Trail.
- Integrate the development with the Iron Horse Trail.

BLOCK G

Block G contains two large parcels, Bishop Ranch 6 and the Toyota Parts Distribution and Training Center, and one smaller parcel adjacent to the freeway which houses a veterinary center. One portion of the Toyota parcel is currently vacant land which has long been considered the City's Housing Element as a potential site for workforce housing.

Intent:

- Encourage the consolidation of the Bishop Ranch offices in the specific plan area to this block, integrated with high quality, high density multifamily residential development.
- Encourage the development of modern larger floor plate regional retail uses which are currently not represented in the City.
- Provide a visual and physical link to the Park Commons in Block D.
- Encourage the integration of support commercial and office uses in the multifamily residential development areas.



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- Internal Streets:** Provide a new Street E from Camino Ramon.
Provide a new Street F linking Camino Ramon and Bishop Drive to increase circulation options and encourage future infill development on the adjacent Bishop Ranch 8 parcel.
- Minimum Streetwall Heights:** 35 feet
- Upper Floor Setbacks:** Daylight Plane Requirement
- Public Spaces:** Park space adjacent to Street E East
- Applicable Street Sections:**
- 4-11: Norris Canyon Road
 - 4-13: Camino Ramon South
 - 4-14: Park Commons South/Transit Center
 - 4-15: Park Commons South
 - 4-16: Street E East
 - 4-17: Street F East
 - 4-18: Service Corridor
- Special Features**
- Active commercial storefronts and building entries are encouraged on Norris Canyon Road.
 - Provide accommodation adjacent to Norris Canyon Road for the relocation of the Transit Center and related parking.
 - Structured parking is strongly encouraged.

STREET STANDARDS

In order to achieve an integrated development of the specific plan area over a number of years by a multiple number of property owners, and for a wide diversity of uses, the plan includes detailed development standards to define street right-of-way widths and dimensions, sidewalk widths and uses, build-to lines for structures, and other conditions such as minimum first floor heights in critical and potential retail locations.

The diagrams and street sections identified in Figure 6.2 include information on the intent and specific requirements for each street corridor in order to implement the Plan Vision. For streets not specifically identified and illustrated in this Chapter, the City shall use these plans and sections along with the Block Development Standards in this Chapter to review development proposal on those streets.

All streets will provide green buffer strips between pedestrian sidewalks or amenity spaces and the adjacent traffic travel lanes or parking spaces. These buffer strips may be continuous or segmented depending on the adjacent street and development conditions. During detailed engineering for the streets, consideration will be given to both normal street landscaping and special paving and landscaping to accommodate stormwater runoff.

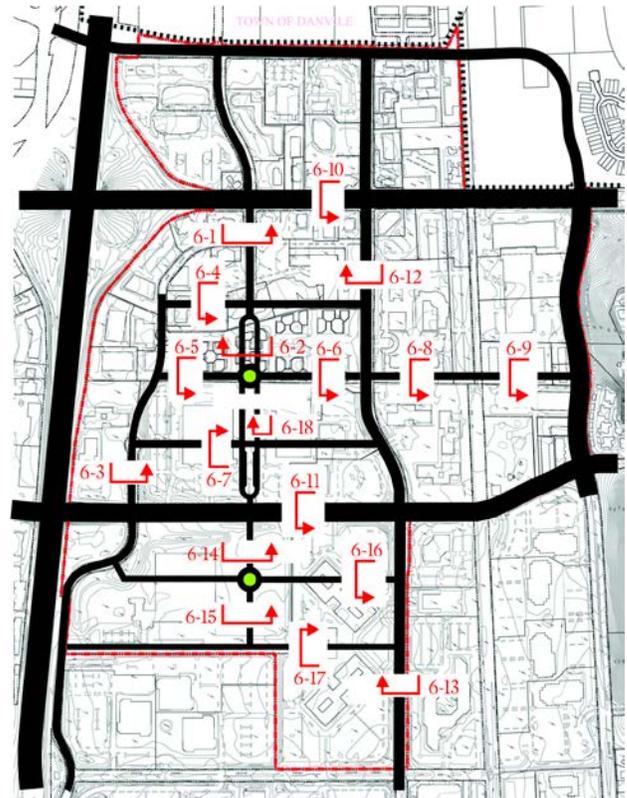
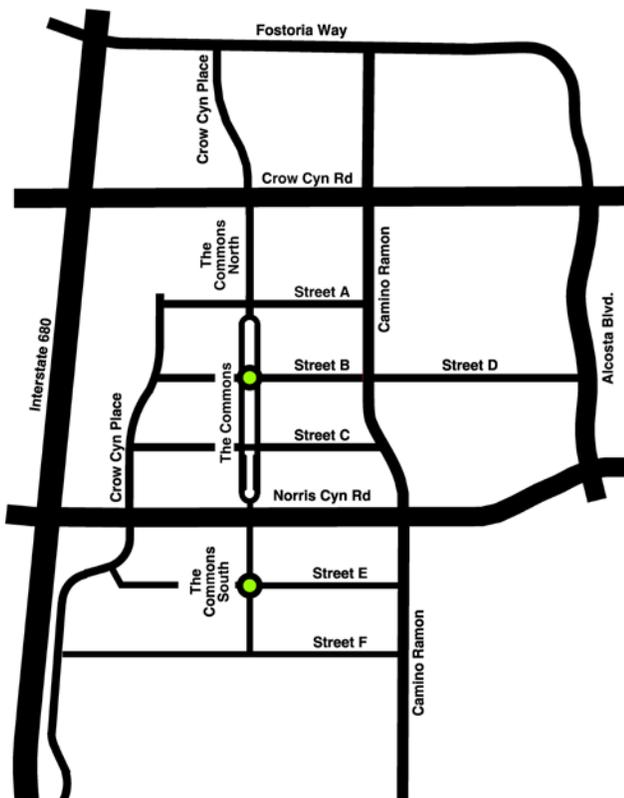


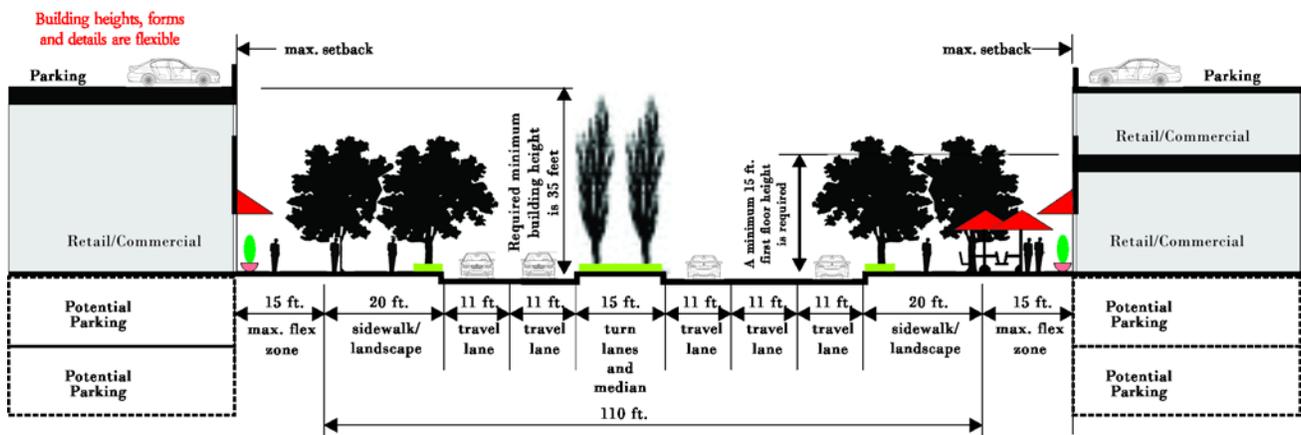
Figure 6.2: Street Framework

SECTION 6-1: THE COMMONS NORTH

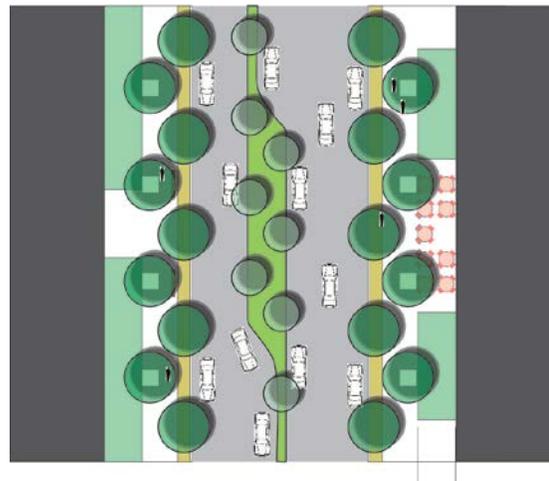
The Commons North will be a major entry to the central block. It will accommodate a significant number of vehicles arriving and exiting from and to east and west San Ramon, as well as Interstate 680.

The street standards are intended to allow adequate distance for arriving motorists to decide on and select their desired destination path: The Commons, the larger floor plate uses along Crow Canyon Place and Norris Canyon Road, or the shared parking structure in the central block.

As the entry to the heart of the North Camino Ramon Specific Plan Area, substantial formal landscaping will be expected in the street median and along its commercial edges.



Right-of-way:	110 feet
On-street parking:	yes
Bike lanes:	no
Sidewalk/Landscape Width:	20 feet
Flex Zone Min.:	10 feet
min. Setback:	10 feet
Max. Setback:	15 feet
Minimum first floor height:	15 feet
Minimum building height:	35 feet
Ground floor active uses:	Yes



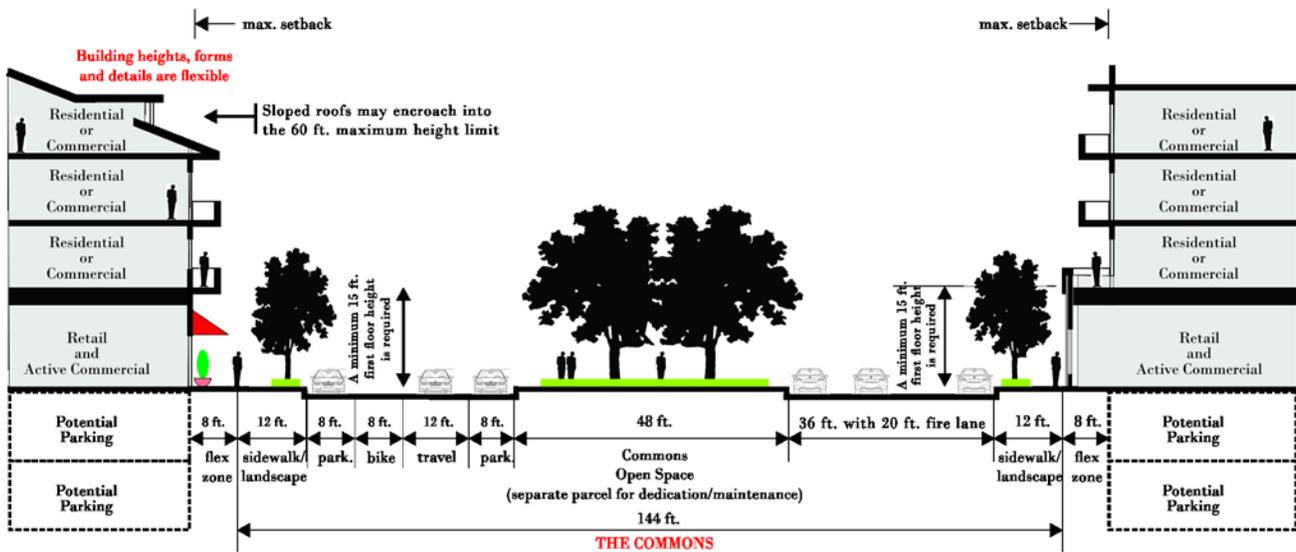
Comments:

- Active Flex Zones are recommended for a minimum of 50% of a project's frontage.
- Landscape buffer is recommended adjacent to street and sidewalks and flex zones to create a pedestrian realm. See Flex Zone Discussion on page 6-32.

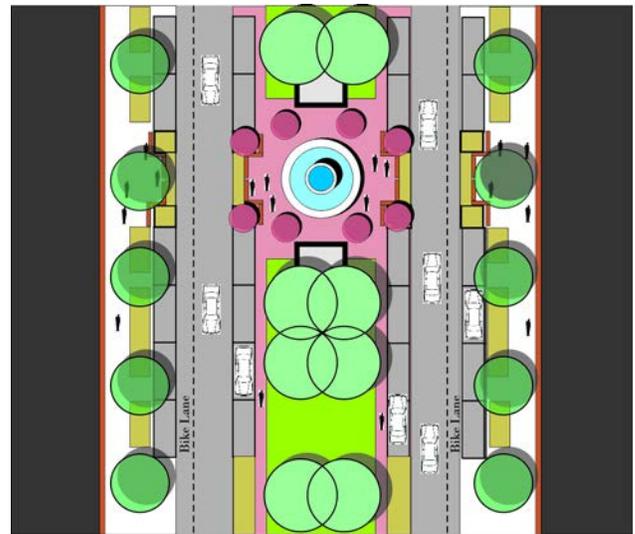
SECTION 6-2: THE COMMONS

The Commons is the heart of the North Camino Ramon Specific Plan Area, and the will provide a strong visual image and identity for the area. It will be a slow traffic local street providing abundant on-street parking for easy access to the retail and commercial service uses along its length.

Ground floor uses and sidewalks will be designed for active uses to enhance the social and economic vitality of the core, and provide an attractive environment to serve the needs of city residents, specific plan area residents, and employees of nearby commercial and medical uses during the day and into the evening.

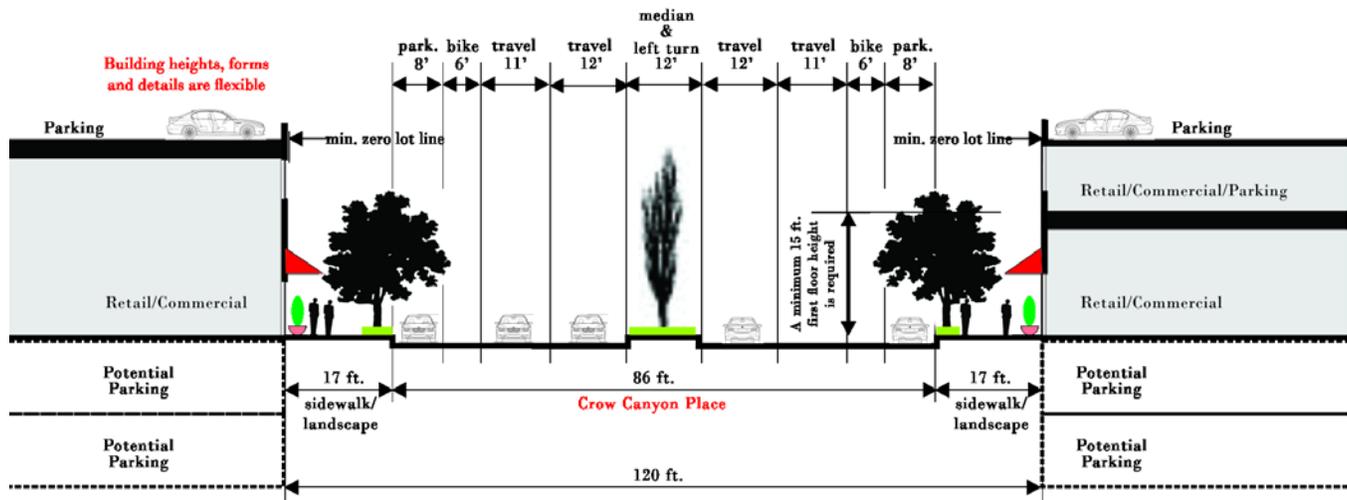


- Right-of-way:** 144 feet
- On-street parking:** yes
- Bike lanes:** yes
- Sidewalk/Landscape Width:** 12 feet
- Flex Zone Min.:** 8 feet
- Min. Setback:** 8 feet
- Max. Setback:** 8 feet
- Minimum first floor height:** 15 feet
- Minimum building height:** 35 feet
- Ground floor active uses:** Yes
- Comments:**

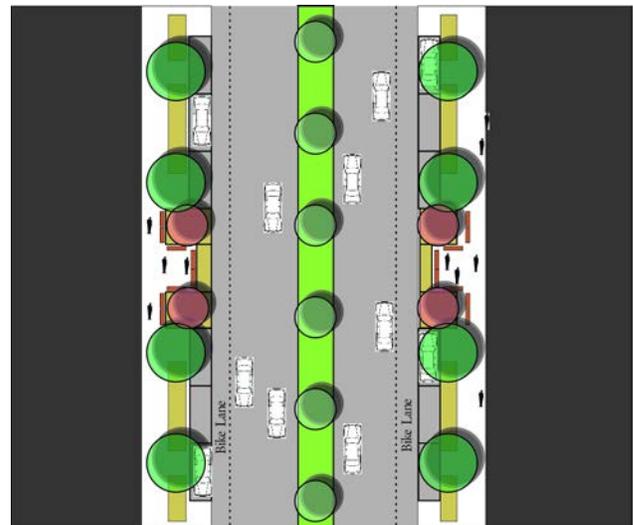


SECTION 6-3: CROW CANYON PLACE

Crow Canyon Place will serve two important functions: as a location for mid- and large-floor plate retail uses and other more automobile-oriented uses, and as an access roadway to similar uses at its intersection with Norris Canyon Road. While more automobile-oriented than other streets within the core area, it will be designed for active and pleasant pedestrian use as an extension of The Commons sidewalk network. Bike lanes are provided to extend those designated in the General Plan on Bishop Drive (to the south) to connect with the “B” Street link to the Iron Horse Trail.



Right-of-way:	120 feet
On-street parking:	yes
Bike lanes:	yes
Sidewalk/Landscape Width:	17 feet
Flex Zone Min.:	none
Min. Setback:	none
Max. Setback:	5 feet
Minimum first floor height:	15 feet
Minimum building height:	none
Ground floor active uses:	Yes



Comments:

- Development along the east side of the street will be expected to reinforce the build-to lines and the strong pedestrian environment of The Commons area with structures built at or within 5 feet of the sidewalk line.
- Development along the east side of the street will be expected to integrate parking with spaces constructed below grade, on upper levels, or surrounded by actively occupied

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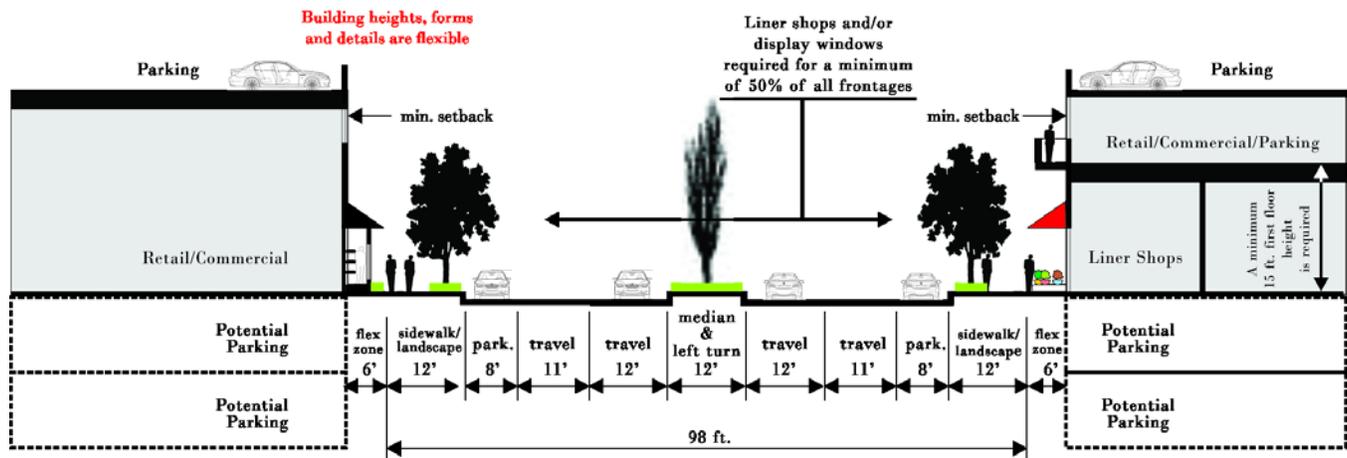
building space. Surface parking fronting onto Crow Canyon Place will not be permitted as part of new development.

- Development along the west side of the street will be expected to reinforce the build-to lines. However, more latitude will be allowed in building and parking placement as part of the development plan review given the specific project elements and consistency with the Plan Vision.

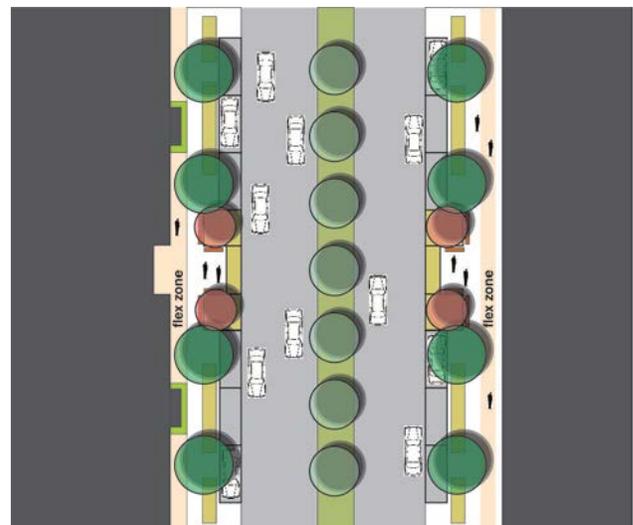
SECTION 6-4: STREET A

Street A will provide a vehicular link between the north entry to the central block at The Commons North and the automobile-oriented uses along Crow Canyon Place. It will also offer access to the shared parking structure.

While additional lanes are provided to accommodate the anticipated travel on this street, sidewalks are planned as an extension of the pedestrian-oriented environment along The Commons.



Right-of-way:	98 feet
On-street parking:	yes
Bike lanes:	No
Sidewalk/Landscape Width:	12 feet
Flex Zone Min.:	6 feet
Min. Setback:	6 feet
Max. Setback:	10 feet
Minimum first floor height:	15 feet
Minimum building height:	none
Ground floor active uses:	Yes

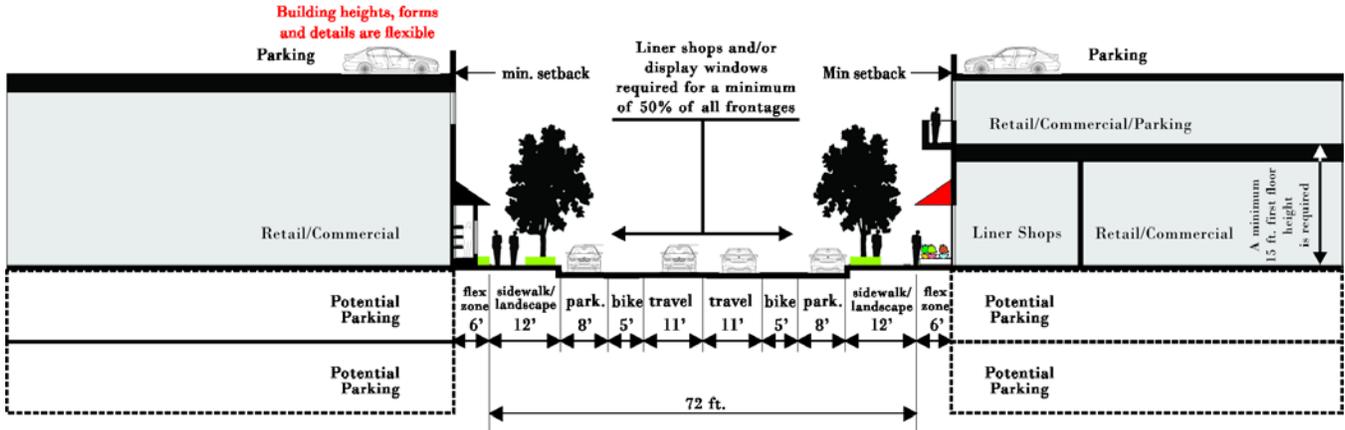


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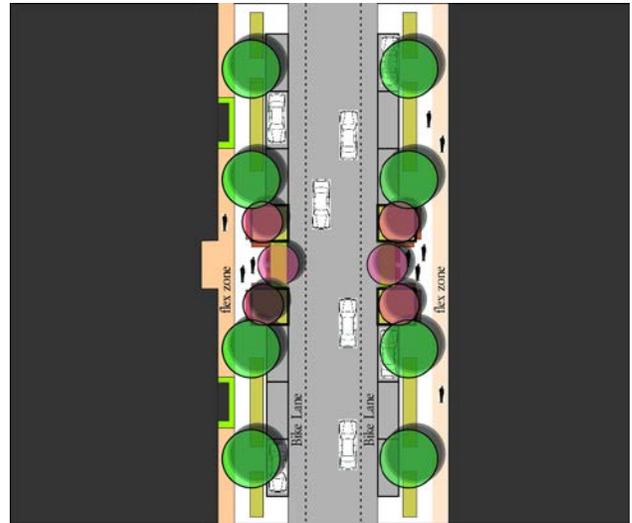
- Liner shops and/or display windows recommended for a minimum of 50% of all frontages.

SECTION 6-5: STREET B WEST

Street B West is a local street with bike lanes provided to link those on Crow Canyon Place and south on Bishop Drive to the Iron Horse Trail. The street also provides a direct vehicular and pedestrian connection between the shared parking structure and Crow Canyon Place.



- Right-of-way:** 72 feet
- On-street parking:** yes
- Bike lanes:** yes
- Sidewalk/Landscape Width:** 12 feet
- Flex Zone Min.:** 6 feet
- Min. Setback:** 6 feet
- Max. Setback:** 10 feet
- Minimum first floor height:** 15 feet
- Minimum building height:** none
- Ground floor active uses:** Yes

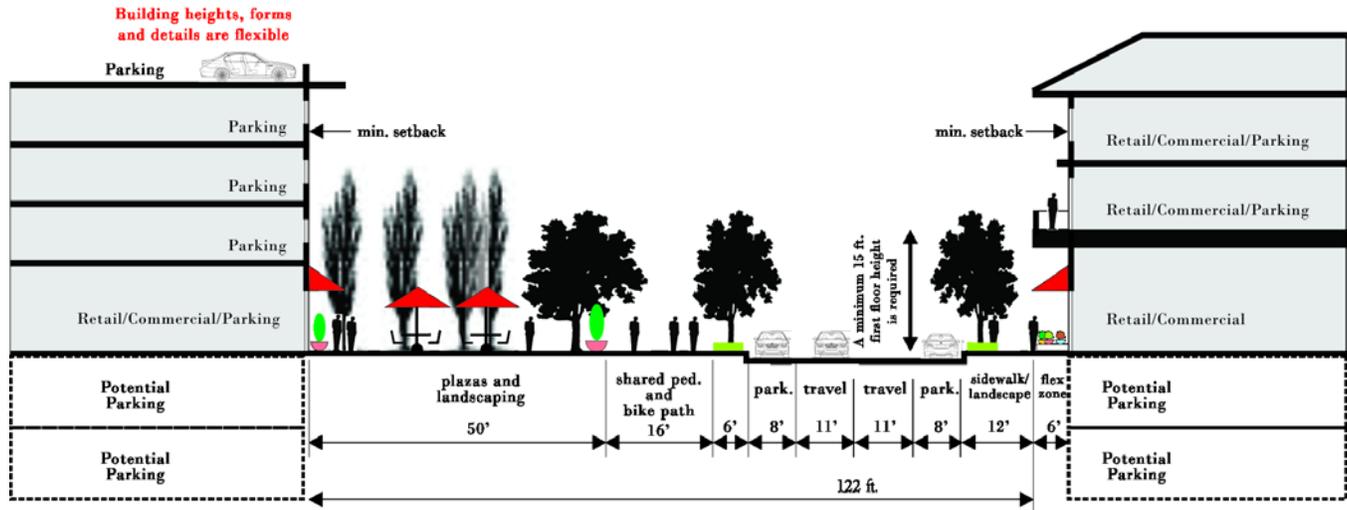


Comments:

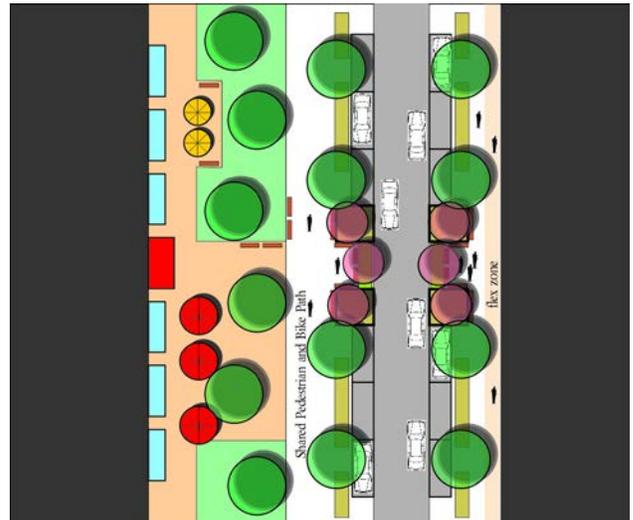
- Liner shops and/or display windows recommended for a minimum of 50% of all frontages.

SECTION 6-6: STREET B EAST

Street B East is a local street adjacent to the shared parking structure and the Iron Horse Trail pedestrian and bicycle path. The street also provides a direct vehicular and pedestrian connection between Crow Canyon Place and the shared parking structure.



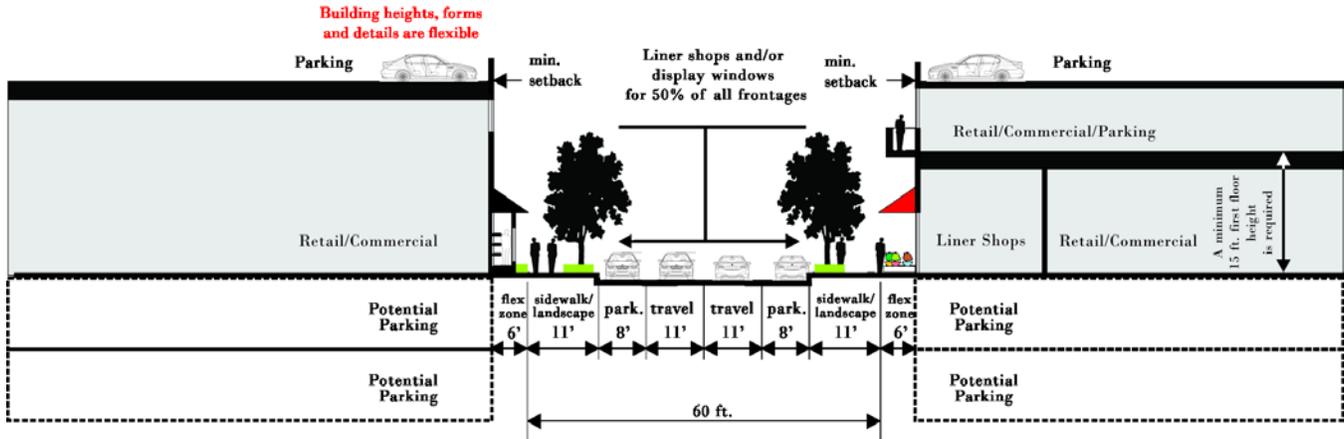
- Right-of-way:** 122 feet
- On-street parking:** yes
- Bike lanes (trail):** yes
- Sidewalk/Landscape Width:** 12 feet
- Flex Zone Min.:** 6 feet
- Min. Setback:** 6 feet
- Max. Setback:** 10 feet
- Minimum first floor height:** 15 feet
- Minimum building height:** none
- Ground floor active uses:** see comments
- Comments:**



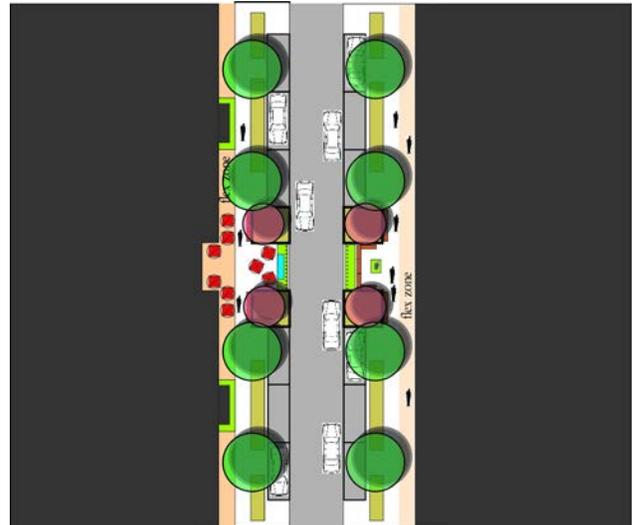
- A 16 foot wide shared pedestrian and bicycle path is required along the north side of the street.
- A 50 foot wide area of plazas and landscaping is required along the north side of the street. Landscaping must be adequate to provide a visual continuation of the landscaped link to the east of Camino Ramon (see section 4-7: D Street West).
- Vehicular access to uses on the north of this street will not be permitted.

SECTION 6-7: STREET C

Street C is a local street with on-street parking. While this street is expected to be constructed for vehicular and pedestrian linkages unless the land use precludes it, some flexibility will be maintained with respect to its precise location consistent with sound practices regarding street intersection spacing.



Right-of-way:	60 feet
On-street parking:	yes
Bike lanes:	no
Sidewalk/Landscape Width:	11 feet
Flex Zone Min.:	6 feet
Min. Setback:	6 feet
Minimum first floor height:	15 feet
Minimum building height:	none
Ground floor active uses:	Yes
Comments:	

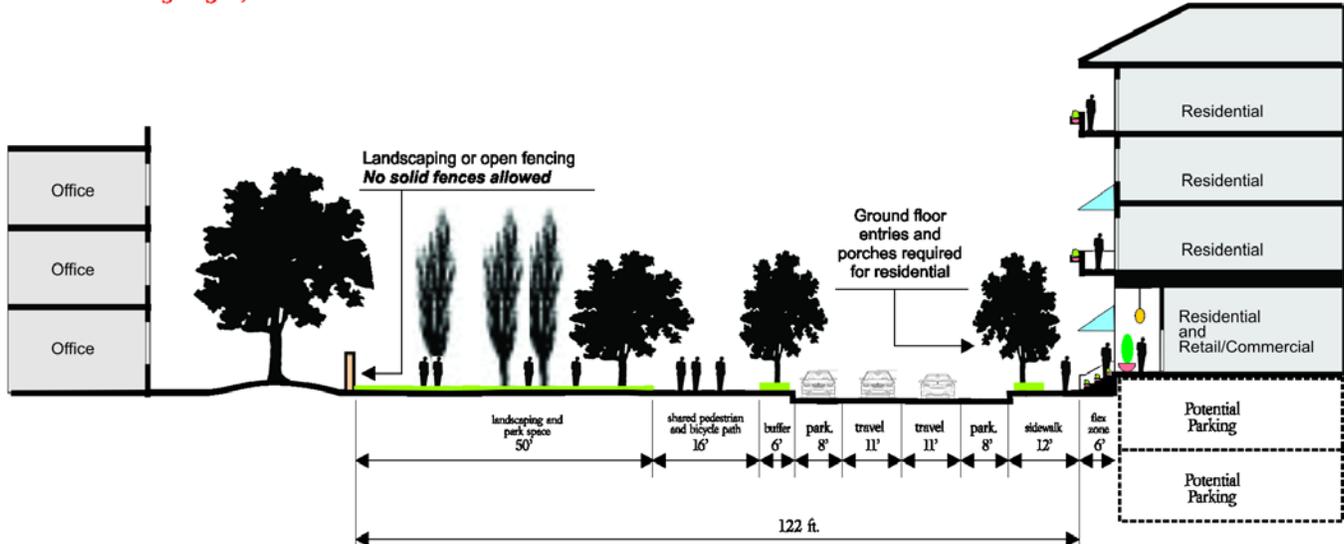


- Liner shops and/or display windows recommended for a minimum of 50% of all frontages.

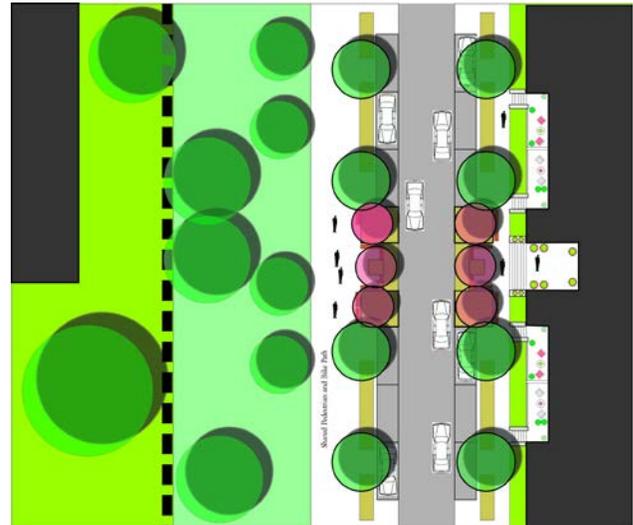
SECTION 6-8: STREET D WEST

Street D West is the westerly portion of a new local street connecting Alcosta Boulevard to Camino Ramon, and providing additional vehicular access to the central block, it also will provide activity to increase the public safety of the Iron Horse Trail Link.

Building heights, forms and details are flexible



Right-of-way:	122 feet
On-street parking:	yes
Bike lanes:	yes
Pedestrian/Bike Path	18 feet
Sidewalk/Landscape Width:	12 feet
Setback for residential:	15 feet
Max. Setback:	none
Flex Zone Min.:	none
Minimum first floor height:	none
Minimum building height:	none
Ground floor active uses:	not required

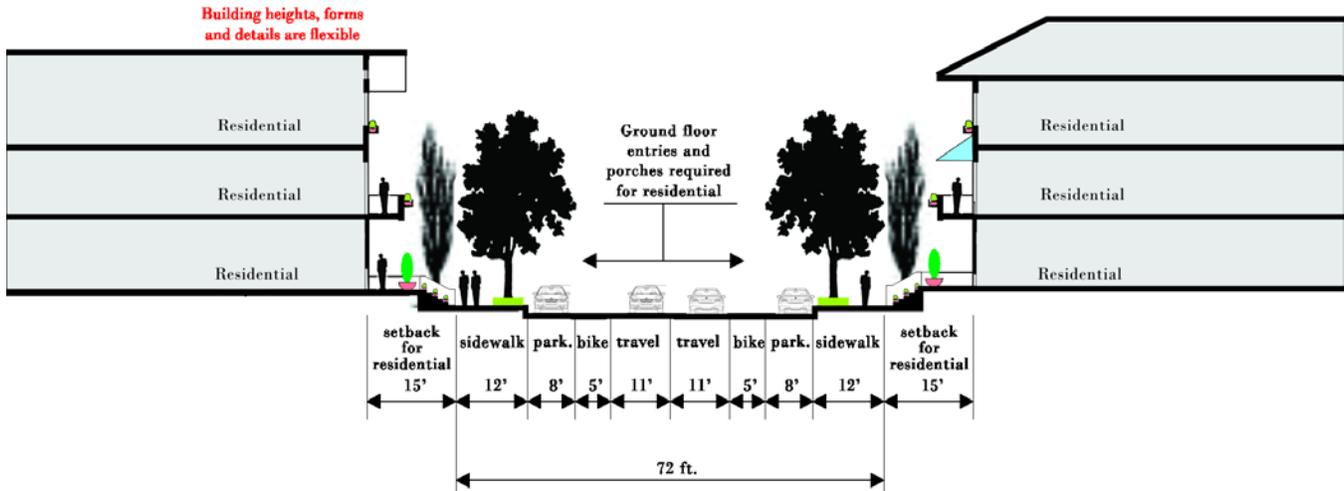


Comments:

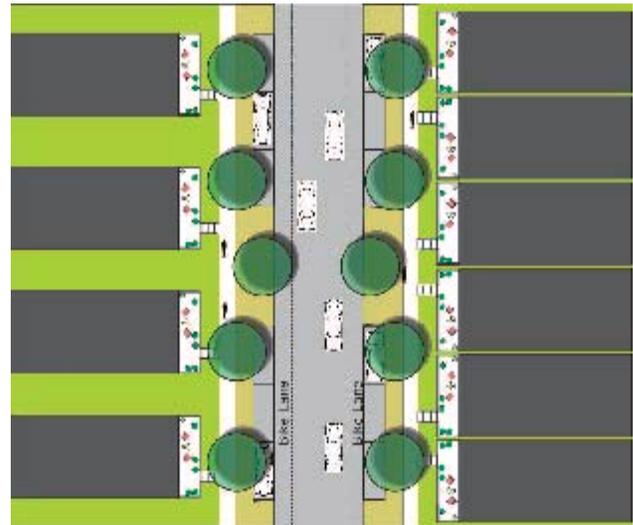
- A 16 foot wide shared pedestrian and bicycle path is required along the north side of the street.
- A 50 foot wide area of park space and landscaping is required along the north side of the street.
- Vehicular access to uses to the north of this street will not be permitted.
- Commercial components of residential developments must adhere to the residential setback; however, may utilize set back area as active Flex Zone space to add to the pedestrian connectivity.

SECTION 6-9: STREET D EAST

Street D East is the easterly portion of a new local street connecting Alcosta Boulevard to Camino Ramon, providing additional vehicular access to the development parcels along its edges, and providing pedestrian and bicycle connections to the Iron Horse Trail and The Commons.



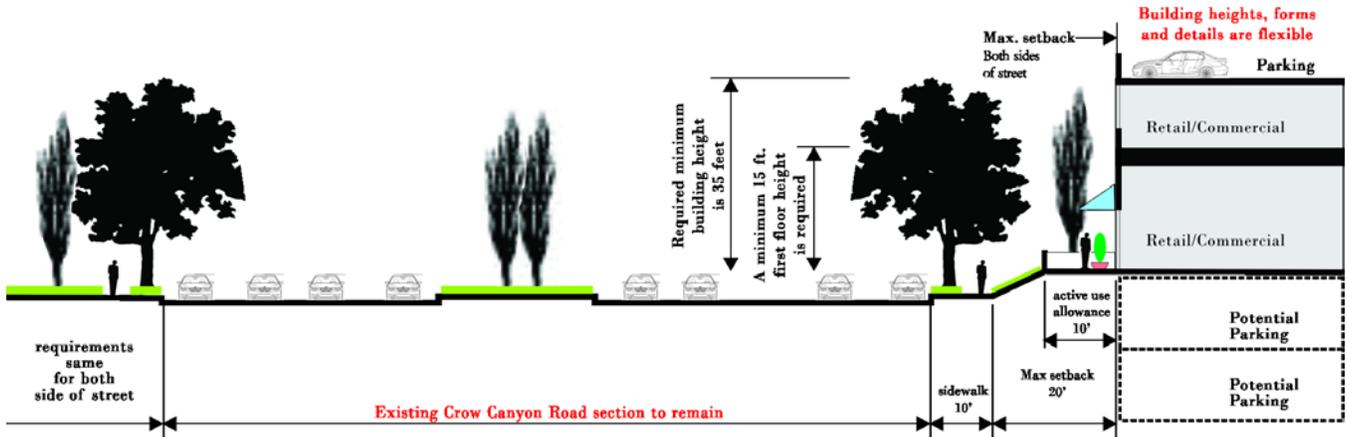
Right-of-way:	72 feet
On-street parking:	yes
Bike lanes:	yes
Sidewalk/Landscape Width:	12 feet
Setback for residential:	15 Feet
Max. Setback:	none
Flex Zone Min.:	none
Minimum first floor height:	none
Minimum building height:	none
Ground floor active uses:	not required
Comments:	



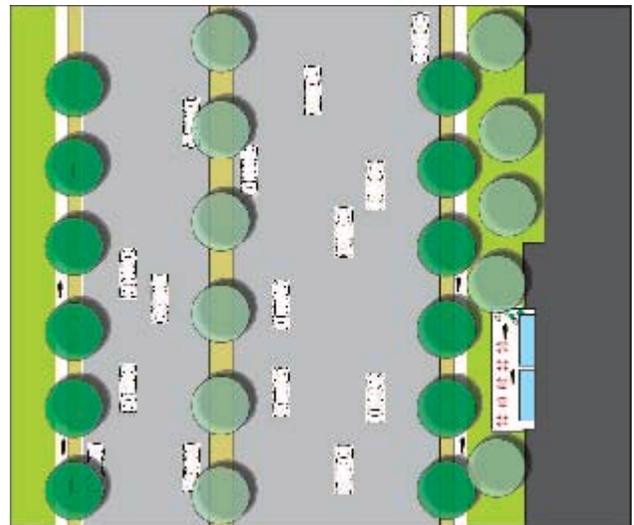
- Commercial components of residential developments must adhere to the residential setback; however, may utilize setback area as active Flex Zone space to add to the pedestrian connectivity.

SECTION 6-10: CROW CANYON ROAD

Crow Canyon Road's importance as a primary entry to the City of San Ramon will be reinforced with structures and ground floor uses that better define the edges of the thoroughfare. The street's current width and profile will remain unaltered.



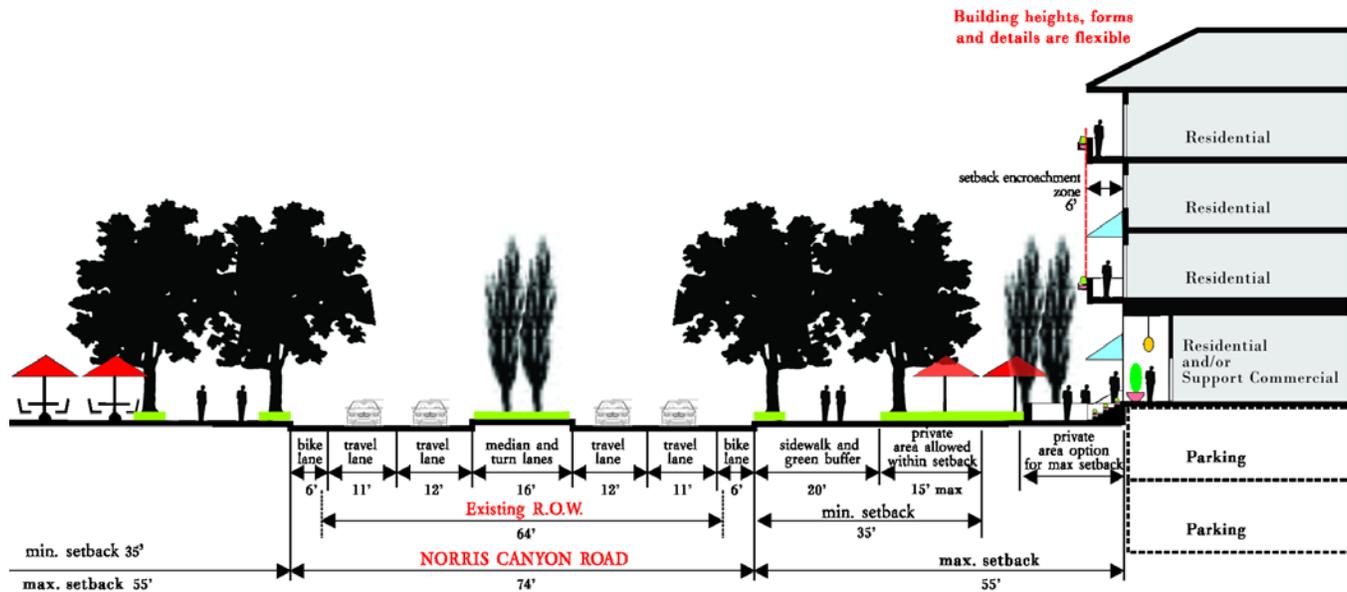
Right-of-way:	Varies
On-street parking:	no
Bike lanes:	no
Sidewalk/Landscape Width:	10 feet
Flex Zone Min.:	None
Min. Setback:	10 feet
Max. Setback:	20 feet
Minimum first floor height:	15 feet
Minimum building height:	35 feet
Ground floor active uses:	Yes
Comments:	



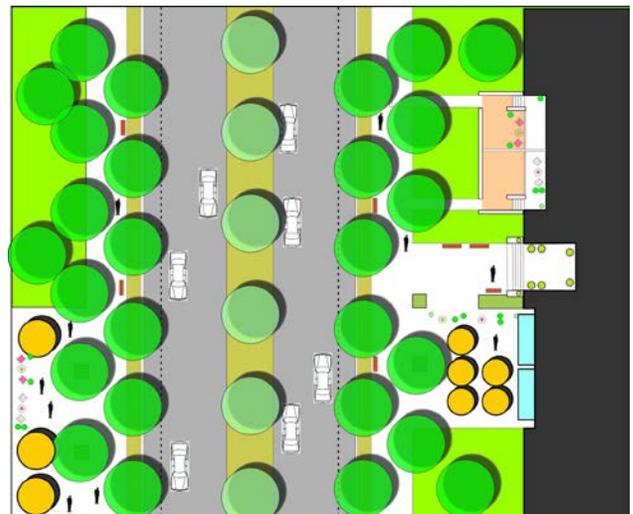
- While a minimum height at the build-to line is required, taller structures are strongly encouraged to enhance the City entry importance of the street.
- 10 foot Flex Zone permitted within adjacent to the proposed buildings.

SECTION 6-11: NORRIS CANYON ROAD

Norris Canyon Road will be widened slightly from its current width to accommodate a landscaped median to enhance the anticipated residential and resident-serving uses along its edge. Active uses, ground floor entries to residential units and buildings, and upper floor balconies to bring life to the street environment will be strongly encouraged to enhance the street's role of providing pedestrian access between nearby employment uses and The Commons.



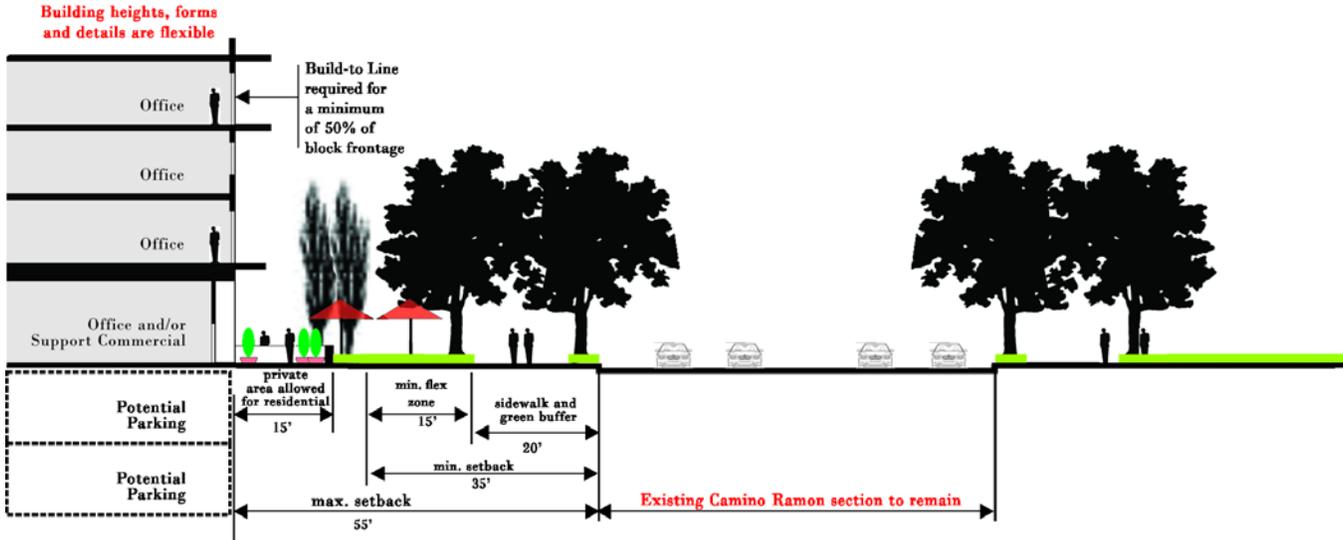
- Right-of-way:** 74 feet
- On-street parking:** no
- Bike lanes:** yes
- Sidewalk/Landscape Width:** 20 feet
- Flex Zone Min.:** 15
- Min. Setback:** 35 feet
- Max. Setback:** 55 feet
- Minimum first floor height:** none
- Minimum building height:** none
- Ground floor active uses:** Yes
- Comments:**



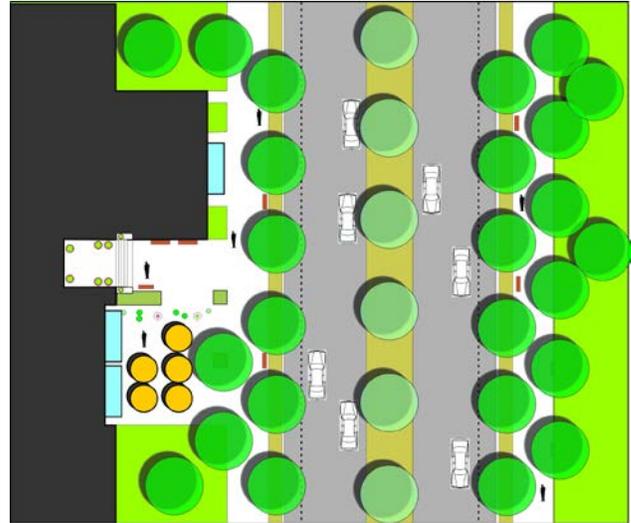
- Sidewalk and landscape to be integrated into the street front design and are not part of the public right of way. Alternative sidewalk alignments may be considered on a case by case basis.
- Active ground floor uses are strongly encouraged along both sides of Norris Canyon Road.
- Soft landscaping with outdoor uses (e.g., dining and gathering spaces) are strongly encouraged to maintain the street's current character.

SECTION 6-12: CAMINO RAMON NORTH

Camino Ramon North will likely provide frontages for a wide variety of uses, including larger structures such as supermarkets, drug stores, single tenant retail buildings, and office buildings. A variety of edge conditions are possible, but the goal is to enhance the landscaping along this street, and to improve its pedestrian environment to encourage walking to the uses that may front on the street.



Right-of-way:	Varies
On-street parking:	no
Bike lanes:	no
Sidewalk/Landscape Width:	20 feet
Flex Zone Min.:	15 feet
Min. Setback:	15feet
Max. Setback:	None
Minimum first floor height:	None
Minimum building height:	None
Ground floor active uses:	Yes

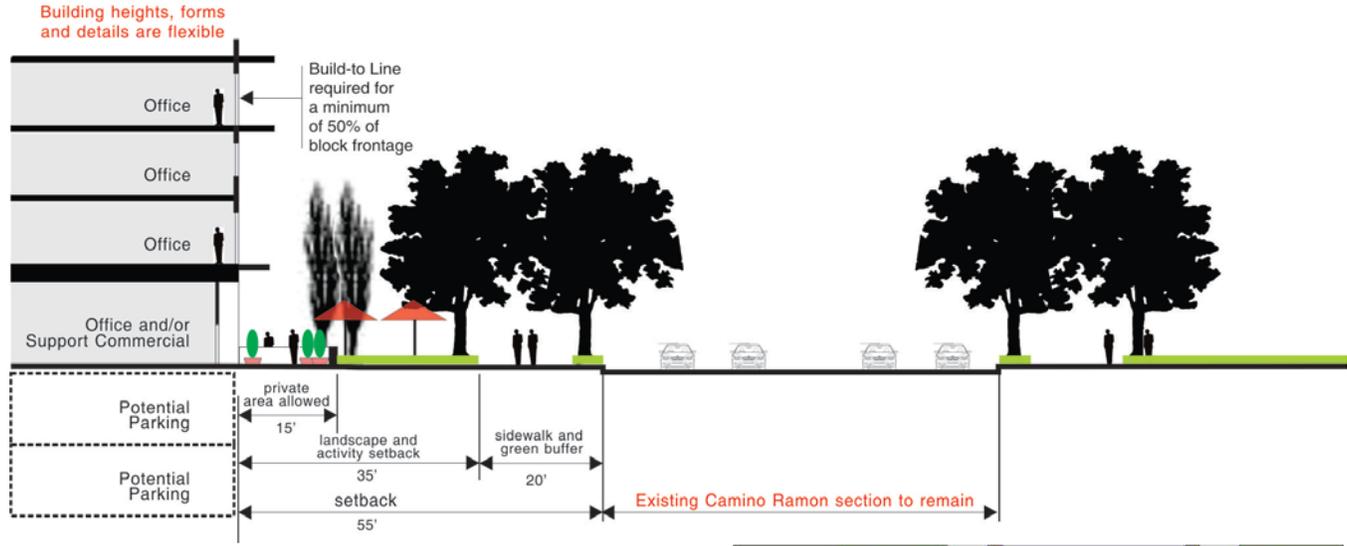


Comments:

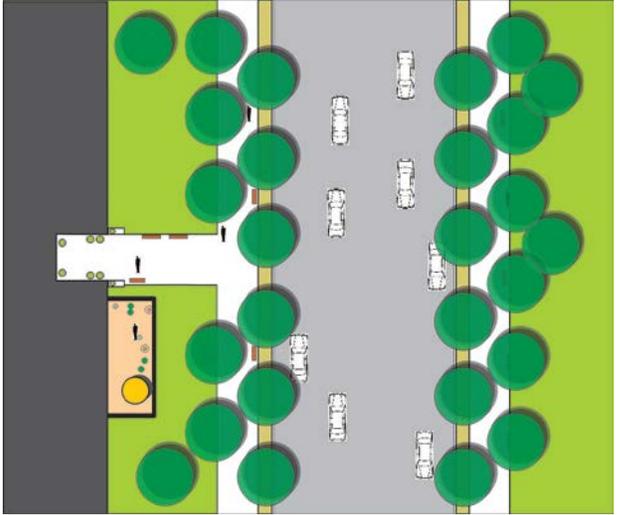
- Sidewalk and landscape to be integrated into the street front design and are not part of the public right of way. Alternative sidewalk alignments may be considered on a case by case basis.
- Reinforcement of edge landscaping is required.
- Sidewalk width includes green buffer but may not include flex space. Flex zones not utilized for active use shall be landscaped.

SECTION 6-13: CAMINO RAMON SOUTH

Camino Ramon South is envisioned as a consolidation of the Bishop Ranch office uses in the specific plan area, combined with residential uses to enliven the area and to support Bishop Ranch businesses, City Center, and The Commons commercial core. Active uses, ground floor entries to residential units and buildings, and upper floor balconies to bring life to the street environment will be strongly encouraged to enhance the street’s role of providing pedestrian access between nearby employment uses and The Commons.



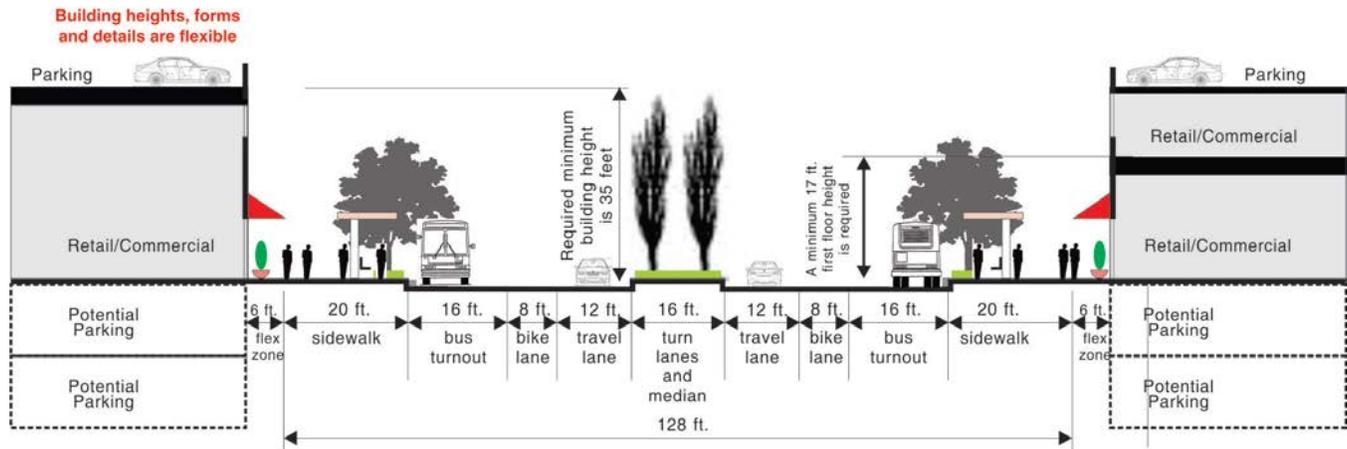
Right-of-way:	Varies
On-street parking:	no
Bike lanes:	no
Sidewalk/Landscape Width:	20 feet
Flex Zone Min.:	15 feet
Min. Setback:	35 feet
Max. Setback:	55 feet
Minimum first floor height:	None
Minimum building height:	None
Ground floor active uses:	Yes



- Comments:**
- Active ground floor uses are strongly encouraged along Camino Ramon.
 - Soft landscaping with outdoor uses (e.g., dining and gathering spaces) are strongly encouraged.
 - Sidewalk and landscape to be integrated into the street front design and are not part of the public right of way. Alternative sidewalk alignments may be considered on a case by case basis.
 - Sidewalk width includes green buffer but may not include flex space. Flex zones not utilized for active use shall be landscaped.

SECTION 6-14: THE COMMONS SOUTH TRANSIT CENTER

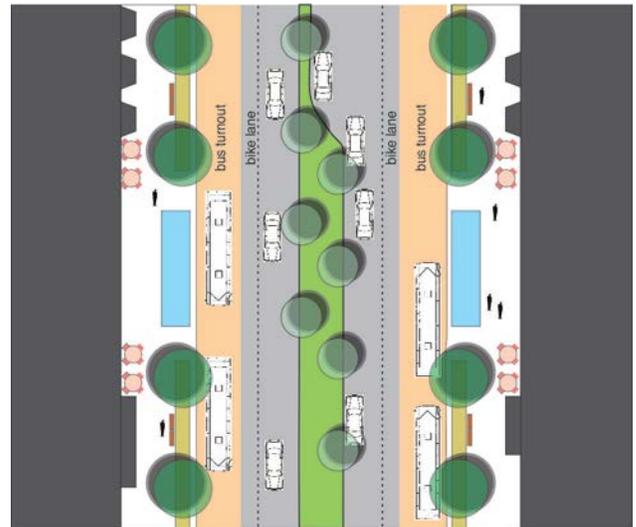
The Commons South, immediately adjacent to Norris Canyon Road, will accommodate the relocated Transit Center, and provide a mix of uses including restaurant, retail, and commercial services to meet the needs of nearby Bishop Ranch office workers and residents in the planned adjacent residential neighborhood. Residential uses, over ground floor commercial uses will be required on the east side of the street to provide a transition to the planned adjacent residential neighborhood. Parking to serve development and the Transit Center are possible on the upper floors on the west side.



Right-of-way:	128 feet
On-street parking:	no
Bike lanes:	yes
Sidewalk/Landscape Width:	20 feet
Flex Zone Min.:	6 feet
Max. Setback:	None
Minimum first floor height:	15 feet
Minimum building height:	35 feet
Ground floor active uses:	Yes

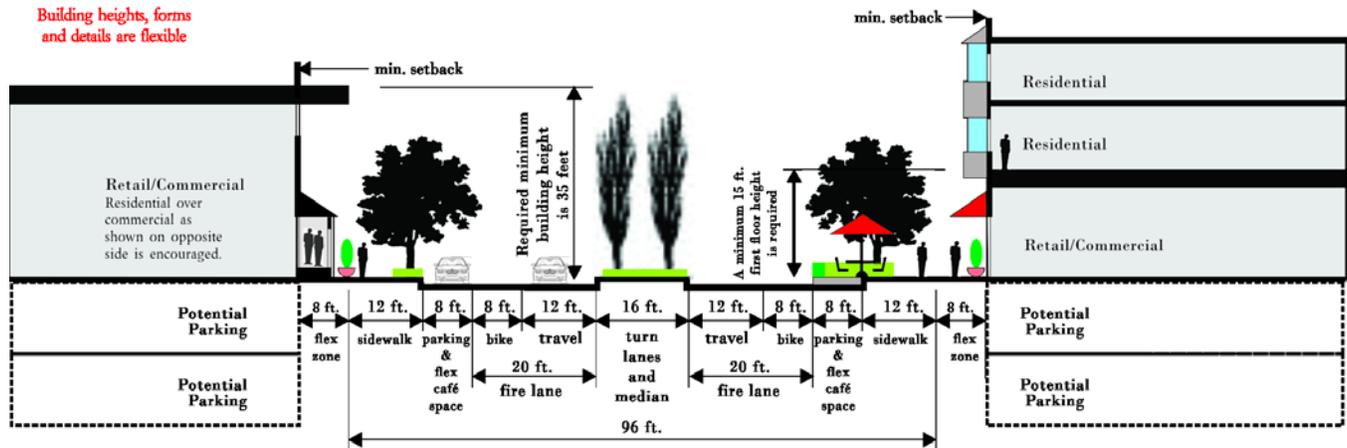
Comments:

- Accommodate an on-street Transit Center adjacent to Norris Canyon Road.
- Provide parking to serve development and transit center.

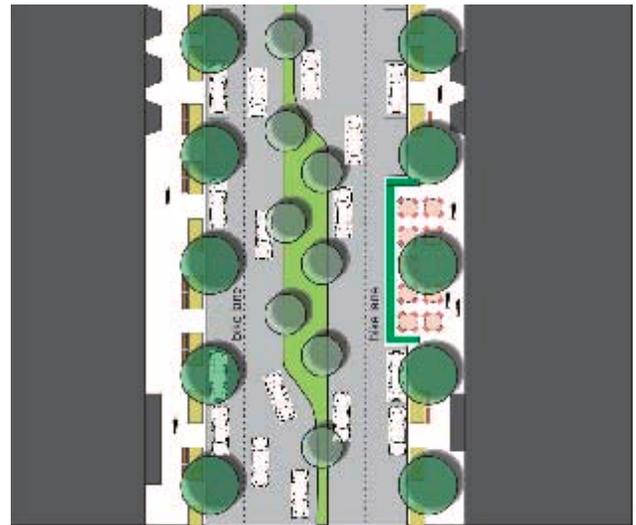


SECTION 6-15: THE COMMONS SOUTH

The Commons South, between the new Streets E and F, will provide a mix of uses including restaurant, retail, and commercial services to meet the needs of nearby Bishop Ranch office workers and residents in the planned adjacent residential neighborhood. Residential uses, over ground floor commercial uses will be required on the east side of the street to provide a transition to the planned adjacent residential neighborhood.



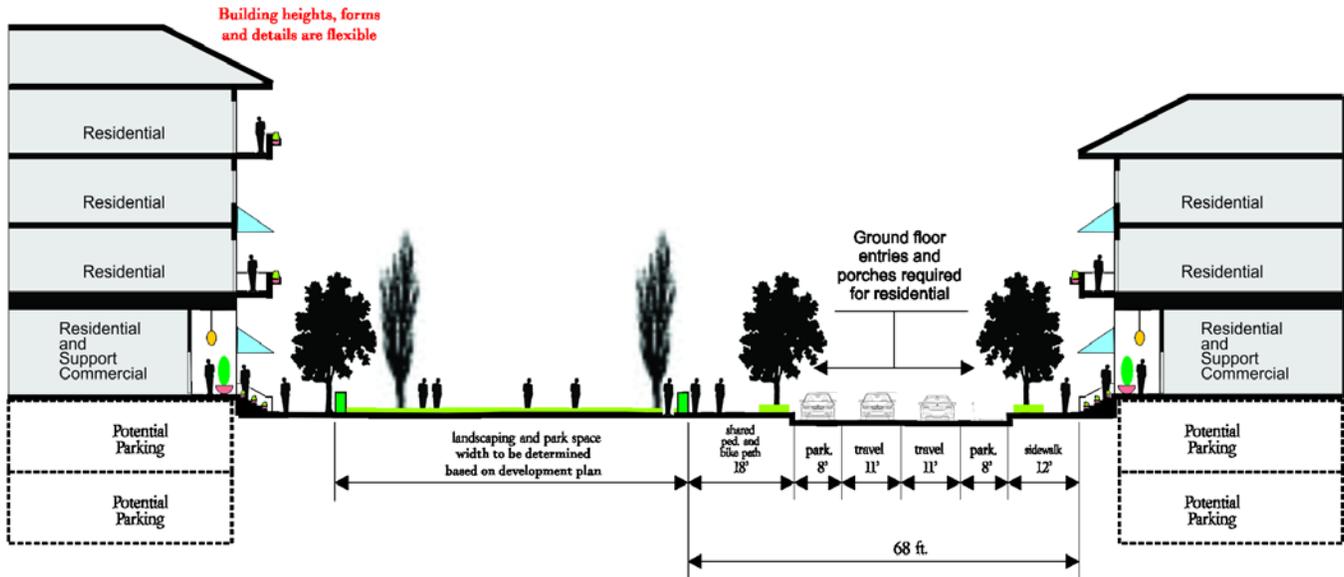
- Right-of-way:** 112 feet
- On-street parking:** yes
- Bike lanes:** yes
- Sidewalk/Landscape Width:** 12 feet
- Flex Zone Min.:** 8 feet
- Max. Setback:** 15 feet
- Minimum first floor height:** 15 feet
- Minimum building height:** 35 feet
- Ground floor active uses:** Yes
- Comments:**



- Active ground floor uses are strongly encouraged.
- Soft landscaping with outdoor uses (e.g., dining and gathering spaces) are strongly encouraged.
- Support commercial uses are encouraged on the ground floors of both office and residential structures to serve the employees and residents.

SECTION 6-16: STREET E EAST

Street E East will provide access to new multifamily residential development, and include park space for both active and passive recreation. Adjacent residential development will be encouraged to provide smaller open spaces and walkways that connect to the park space along the street. On-street parking will provide convenient parking for residential guests and community residents using the open space.



Right-of-way:	68 feet
On-street parking:	yes
Bike lanes:	yes
Sidewalk/Landscape Width:	12 feet
Pedestrian/Bike Path	18 feet
Flex Zone Min.:	None
Residential Setback:	20 Feet
Max. Setback:	20 feet
Minimum first floor height:	none
Minimum building height:	none
Ground floor active uses:	Yes

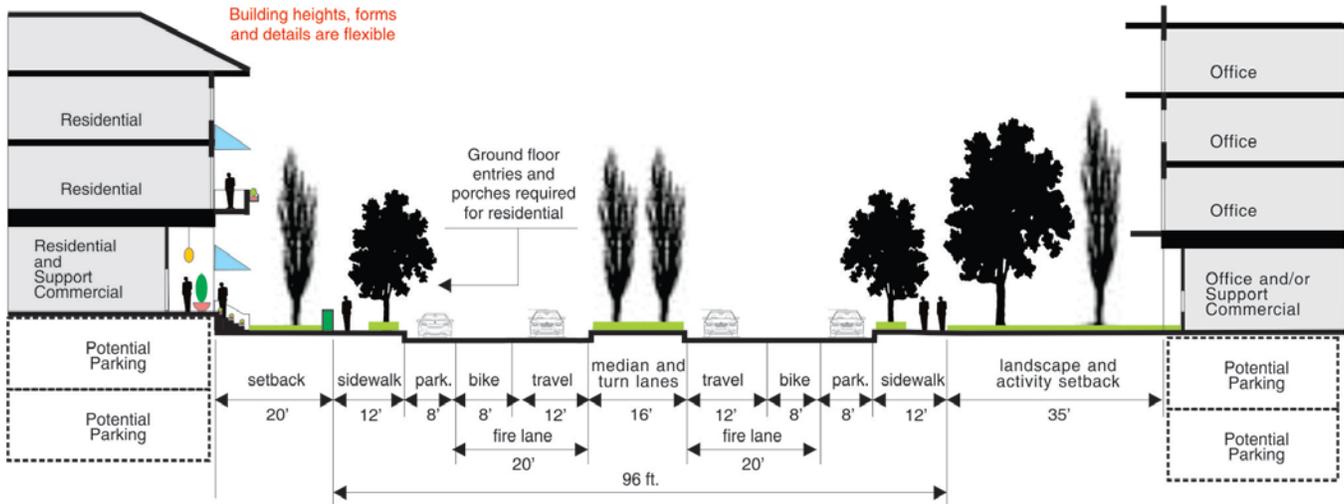


Comments:

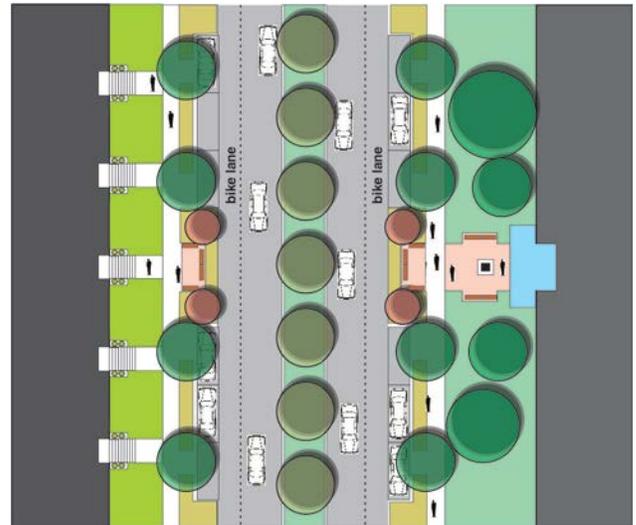
- Shared pedestrian and bicycle path is required.
- Landscaping and park space is required along the street frontage. The width will be determined based on the development plan for the adjacent parcels.

SECTION 6-17: STREET F EAST

Street F East will provide access to new multifamily residential development, and include park space for both active and passive recreation. Adjacent residential development will be encouraged to provide smaller open spaces and walkways that connect to the park space along the street. On-street parking will provide convenient parking for residential guests and community residents using the open space.



Right-of-way:	96 feet
On-street parking:	yes
Bike lanes:	yes
Sidewalk/Landscape Width:	12 feet
Residential setback:	20 feet
Office setback:	35 feet
Max. Setback:	35 feet
Minimum first floor height:	none
Minimum building height:	none
Ground floor active uses:	Yes

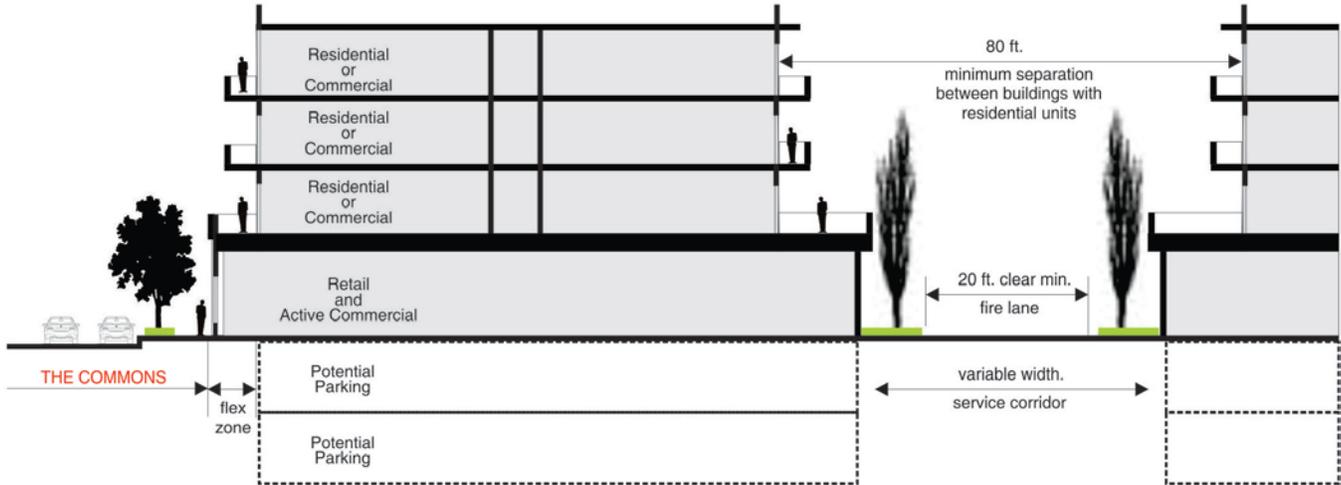


Comments:

- Support commercial uses are encouraged on the ground floors of both office and residential structures to serve the employees and residents.
- Landscape and activity setbacks are required along all office frontage to buffer residential neighborhoods from larger office buildings.

SECTION 6-18: SERVICE CORRIDOR

Service corridors will be required at the rear of buildings fronting on The Commons, including The Commons North and The Commons South, to provide service delivery and trash removal access for commercial uses. Driveway access must be wide enough to accommodate a fire lane. A minimum separation of 80 feet is required between buildings where any one of the structures contains residential units.



SIDEWALK STANDARDS

The treatment of sidewalks within the specific plan area is critical to achieving the goal of an environment that is pedestrian friendly, and encourages walking instead of driving for short trips. They will strongly influence the perception and image of the area, and are considered a part of the area’s public open space system.

Major sidewalks are divided into three zones:

- **Amenity Zone**

Landscaping, rain gardens, paving, benches, bike racks, and other street furniture will provide a buffer between pedestrians and adjacent parked cars and moving traffic.

- **Pedestrian Zone**

A clear path will be maintained at sufficient width to allow side-by-side walking.

- **Flex Zone**

Varying in width according to location and anticipated volume of pedestrian traffic, the flex zone is intended to promote visual diversity and interest along street frontages through a variety of private building and activity uses.

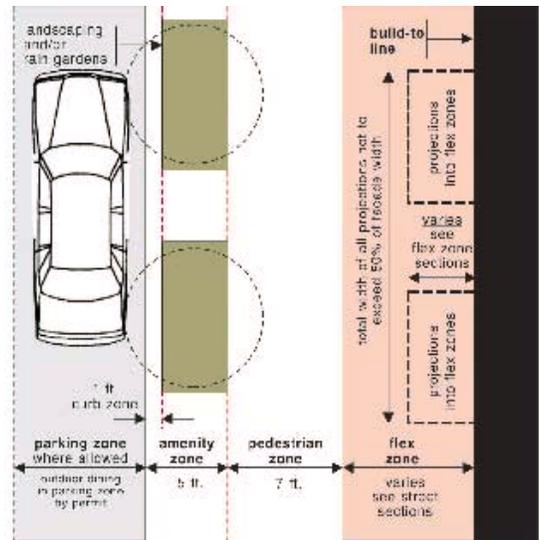
Sidewalks, street trees, and planting strips are required along all streets in the Specific Plan Area.

FLEX ZONES

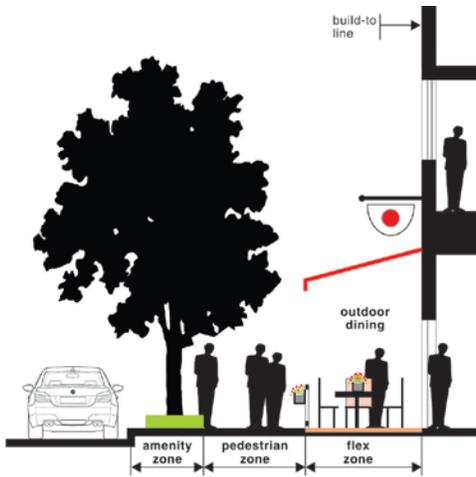
Flex zones are located on development parcels along selected street frontages, as indicated on the street standards sections contained in this chapter. They vary in width according to location and anticipated uses.

Building elements extending into the flex zone are limited to a maximum of 50% of the building’s facade width with the remainder of the area reserved for outdoor activities (e.g., outdoor dining), landscaping, potted plants and trees, special paving, and benches and other pedestrian-friendly street furniture.

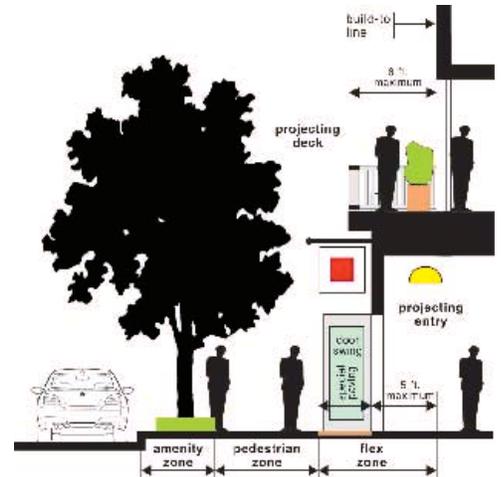
A number of appropriate building and activity uses within the flex zone, along with standards for maximum projections, are shown on the following pages. Other building elements and uses may be considered on a case-by-case basis.



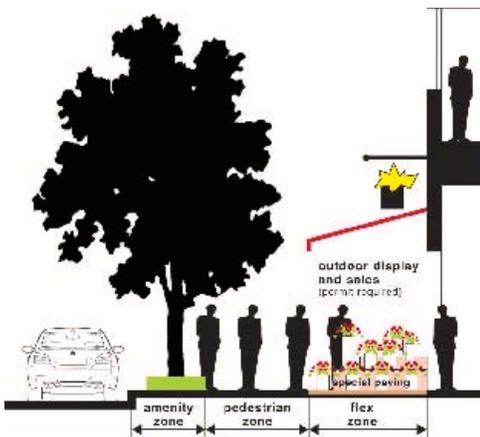
Sidewalk Zones



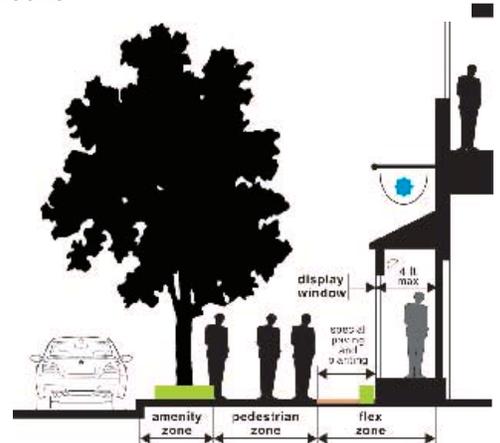
Flex Zone: Outdoor dining



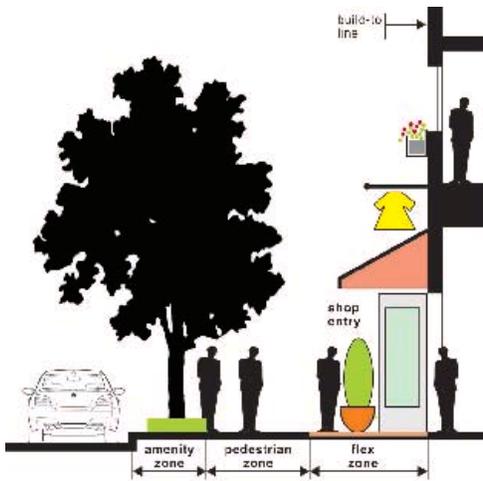
Flex Zone: Projecting entries and decks



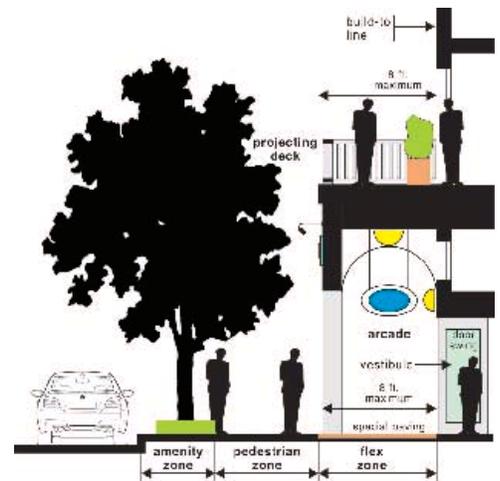
Flex Zone: Outdoor display



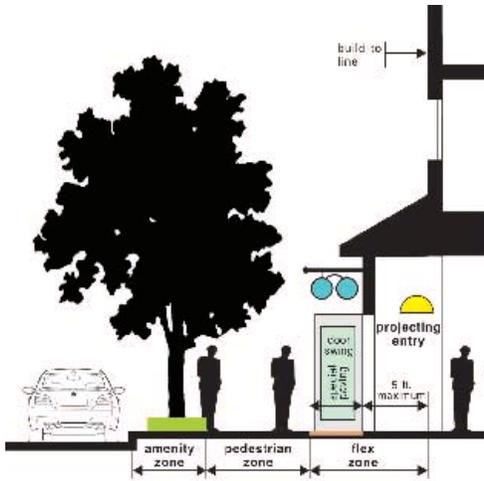
Flex Zone: Display window



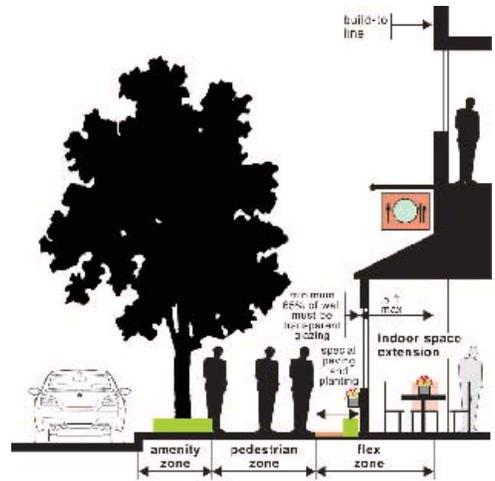
Flex Zone: Projecting entry



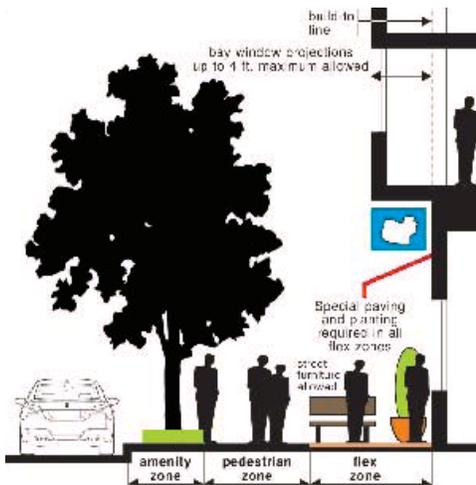
Flex Zone: Arcade



Flex Zone: Shop entry



Flex Zone: Indoor space extension



Flex Zone: seating and landscape

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